

Post & Courier

CHARLESTON COUNTY COUNCIL COMPREHENSIVE PLAN AND ZONING AND LAND DEVELOPMENT REGULATIONS ORDINANCE (ZLDR) AMENDMENTS PUBLIC HEARING **Tuesday, January 19, 2021 at 6:30 PM**

Charleston County Council will hold a public hearing on proposed amendments to the Charleston County Comprehensive Plan and Zoning and Land Development Regulations Ordinance to incorporate the Johns Island Maybank Highway Corridor Overlay Zoning District beginning at 6:30 p.m., Tuesday, January 19, 2021, in Council Chambers (second floor of the Lonnie Hamilton, III, Public Services Building, located at: 4045 Bridge View Drive, North Charleston, SC 29405). The complete packet can be found online at: <https://www.charlestoncounty.org/departments/zoning-planning/projects-maybank-main.php>. The meeting will be livestreamed at: <https://www.charlestoncounty.org/departments/county-council/cctv.php>. Public comments may be made in person, or written comments may be emailed to CCPC@charlestoncounty.org or mailed to the address listed above by noon on Tuesday, January 19. Contact the Zoning and Planning Department at (843)202-7200 or CCPC@charlestoncounty.org for additional information.

This Public Notice is in accordance with Section 6-29-760 of the Code of Laws of South Carolina.

Kristen L. Salisbury
Clerk of Council

Proposed Amendments to the Charleston County Comprehensive Plan and ZLDR to incorporate the Johns Island Maybank Hwy Corridor Overlay Zoning District (JO-MHC-O)

- Case History**
- Presentation**
- Proposed Amendments**
- Public Input**

**JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR
OVERLAY ZONING DISTRICT
CASE HISTORY**

Planning Commission Workshop: January 13, 2020
Planning Commission Workshop and Meeting: October 12, 2020
Planning Commission Workshop and Meeting: November 9, 2020
Public Hearing: January 19, 2021
PPW Committee: January 28, 2021
First Reading: February 2, 2021
Second Reading: February 16, 2021
Third Reading: March 9, 2021

Background

The MHC-O, Maybank Highway Corridor Overlay Zoning District, was first established as part of the Charleston County Zoning and Land Development Regulations Ordinance (ZLDR) when it was adopted on November 20, 2001. The MHC-O included only unincorporated properties along Maybank Highway located on Johns Island. In 2006, the MHC-O district was amended to implement the recommendations of the 2003 *Charleston County Comprehensive Plan Five-Year Review*. In 2018, the *10-Year Comprehensive Plan Update* was adopted, and includes the priority recommendation to coordinate with the City of Charleston to review the MHC-O District and extend the overlay boundary onto James Island. The JO-MHC-O, Johns Island Maybank Highway Corridor Overlay Zoning District, implements that recommendation.

The JO-MHC-O project began in July 2018 with a focus on community input. Public outreach included a community survey resulting in over 1,700 responses, four focused stakeholder meetings, and four rounds of on-location public workshops attended by hundreds of Johns Island residents. The JO-MHC-O divides the corridor into two districts: (1) the Mixed Use District, intended for higher intensity commercial development at the three major nodes along the corridor; and (2) the Limited Commercial District, intended for service and neighborhood commercial uses. It also addresses vehicle access issues, enhances right-of-way buffer and signage requirements, and creates more consistent land use and density/intensity and dimensional standards.

The JO-MHC-O is the culmination of a two-year City of Charleston and Charleston County collaborative planning process including input from hundreds of residents and property owners, business owners, and stakeholders. Below is a summary of the project timeline to date:

- January 6, 2015: County Council adopted the Comprehensive Plan Five-Year Review, including the priority recommendation to review the MHC-O District and extend the overlay boundary onto James Island.
- July 31, 2018: 219 community members attended the first Kick-Off Community Workshop at the Berkeley Electric Cooperative Conference Room
- August 23, 2018: 141 community members attended the second Kick-Off Community Workshop at St. Johns High School.
- August-November 2018: Received 1,725 responses to the JO-MHC-O Community Survey.
- November-December 2018: Held three individual stakeholder meetings:
 - November 28, 2018 Transportation and Drainage Stakeholder Meeting: Included representatives from SCDOT District 6, Town of Kiawah Island, Town of Seabrook Island, and the respective City of Charleston and Charleston County Zoning and Planning, Public Works, and Transportation Departments.

- November 29, 2018 Community Stakeholder Meeting: Included representatives from various community organizations, neighborhood associations, James Island Public Service District, EMS, CCSD, and city and county zoning and planning staff.
- December 4, 2018 Business Owner Stakeholder Meeting: Included representatives from the business community along the corridors of Maybank Highway on James Island, and Maybank Highway and Main Road on Johns Island.
- February 29, 2019: 103 community members attended the third workshop.
- June 17, 2019: 72 community members attended the fourth workshop.
- October 24, 2019: 70 community members attended the fifth workshop.
- January 13, 2020: The first Planning Commission workshop was held.
- March 2020: City and County Staff facilitated Planning Commission member site visits to view and discuss the Johns Island Maybank Highway corridor and elements of the draft JO-MHC-O.
- March 2020 – Present: City and County staff held multiple meetings to finalize the JO-MHC-O.
- To date, City and County staff have met 19 times to discuss, review, and revise the JO-MHC-O.

The proposed amendments to the Comprehensive Plan and the ZLDR to incorporate the JO-MHC-O are attached.

COMPREHENSIVE PLAN AMENDMENTS

In summary, the proposed Comprehensive Plan amendments:

- Amend Chapter 3.1 as follows:
 - Update Map 3.1.4: Future Land Use to amend the JO-MHC-O;
 - Update Map 3.1.5: Urban/Suburban Future Land Use Detail to amend the JO-MHC-O;
 - Incorporate Map 3.1.8: Johns Island Maybank Highway Corridor Overlay Zoning District;
 - Incorporate Map 3.1.19: Johns Island Maybank Highway Corridor Infrastructure and Connectivity Map.
- Amend Chapter 3.9 as follows:
 - In Table 3.9.1, update the Project Status column to show “Completion in 2020”;
 - Remove the Maybank Highway Corridor Overlay Zoning District project from Table 3.9.2.

APPROVAL CRITERIA

Pursuant to Article 3.2.6 of the Charleston County Zoning and Land Development Regulations Ordinance, *Comprehensive Plan* Amendments may be approved by the County Council only if they determine that the proposed amendment is consistent with the overall purpose and intent of the Comprehensive Plan and that any one of the following criteria has been met:

- A. There was a significant error in the original Comprehensive Plan adoption;
Response: N/A
- B. In adopting the Comprehensive Plan, the County Council failed to take into account facts, projections or trends that were reasonably foreseeable to exist in the future;
Response: N/A
- C. Events, trends, or facts after adoption of the Comprehensive Plan have changed the County Council's original findings made upon plan adoption;
Response: N/A

D. Events, trends, or facts after adoption of the Comprehensive Plan have changed the character or condition of an area, making the proposed amendment necessary;

Response: The Maybank Highway Corridor Overlay Zoning District was developed in coordination with the residents of Johns Island and the City of Charleston in the late 1980s/early 1990s. Since that time, many changes affecting this corridor have taken place, including:

- *The City of Charleston revised their Maybank Highway Corridor Overlay Zoning District, adopting land use, density, and site design standards that differ from those adopted by Charleston County; and*
- *Charleston County Council approved the construction of significant transportation improvements in the area, including widening Maybank Highway and incorporating bicycle and pedestrian ways, creating a pitchfork road design network north and south of Maybank Highway on the eastern side of River Road to manage traffic coming onto the island from James Island, and the final leg of Interstate 526 from West Ashley, across Johns Island, and onto James Island.*

These amendments implement the Comprehensive Plan strategy to review this Overlay Zoning District in light of the changes described above and work with the public and the City of Charleston to make revisions as appropriate.

E. The proposed Comprehensive Plan Amendment is requested pursuant to and complies with Art.3.17, Developments of County Significance; or

Response: N/A

F. The proposed Comprehensive Plan Amendment is consistent with the comprehensive plan future land use recommendations of adjacent municipalities that have adopted extra-territorial jurisdiction for the subject parcel(s).

Response: N/A

Staff Recommendation – Comprehensive Plan Amendments: Approval

The proposed amendments meet the criteria of ZLDR Art. 3.2.6.

ZLDR AMENDMENTS

In summary, the proposed ZLDR amendments:

- Amend Article 5.4, MHC-O, Maybank Highway Corridor Overlay Zoning District as follows:
 - Creates two new districts: The Mixed Use District and the Limited Commercial District;
 - Improves pedestrian safety by requiring a 10-foot separated sidewalk the Mixed Use District and a 10-foot separated multi-use path in the Limited Commercial District;
 - Includes incentives for dedication of property for a connector road and drainage system;
 - Limits individual building footprints to 7,500 square feet and building height to 35 feet or 2.5 stories in the Limited Commercial District and 15,000 square feet and 55 feet/5 stories in the Mixed Use District;
 - Requires freestanding signs;
 - Improves vehicle access management; and
 - Limits density and lot area calculations to highland areas only (does not include freshwater wetland or OCRM Critical Line Area).
- Amend other sections of the ZLDR as follows:
 - ZLDR Sec. 8.4.2.A.4.i, Preliminary Plat Application Requirements, and ZLDR Sec. 8.5.2.B.9, Final Plat Application Requirements: To clarify that a United States Army Corps of Engineers (USACE) jurisdictional determination is required for approval of a subdivision plat application as required for the Johns Island Maybank Highway Corridor Overlay Zoning District.

- ZLDR Sec. 9.5.4, Table 9.5.4.A.3, Buffer Types by Roadway: Amend the table to change “Maybank Highway Corridor Overlay Zoning District” to “Maybank Highway Corridor Overlay Zoning District (Johns Island), and change footnote 2 to state: “Buffer type shall be as described in the Johns Island Maybank Highway Corridor Overlay Zoning District.”

APPROVAL CRITERIA

Pursuant to Article 3.3.6 of the Charleston County Zoning and Land Development Regulations Ordinance, text amendments to the ordinance may be approved if the following approval criteria have been met:

- A. The proposed amendment corrects an error or inconsistency or meets the challenge of a changing condition.

Response: The proposed overlay zoning district meets the challenge of the changing conditions of the Johns Island Maybank Highway corridor by improving pedestrian mobility, requiring standards for safer and more efficient vehicle access, enhancing right-of-way buffer and signage requirements, and adding more restrictive density/intensity and dimensional standards.

- B. The proposed amendment is consistent with the adopted Charleston County Comprehensive Plan and goals as stated in Article 1.5;

Response: The proposed overlay zoning district is consistent with the adopted Charleston County Comprehensive Plan and goals as stated in Article 1.5 as it implements the Comprehensive Plan’s priority recommendation to review and revise the Johns Island Maybank Highway Corridor Overlay Zoning District.

- C. The proposed amendment is to further the public welfare in any other regard specified by County Council;

Response: The proposed overlay zoning district furthers the public welfare by: improving the safety and efficiency of vehicle, pedestrian, and bicycle movement in the corridor; enhancing right-of-way buffer and signage requirements; and addressing drainage issues through Special Protection Area stormwater design criteria.

Public Input: All public input received to date is included in this packet.

Staff Recommendation – ZLDR Amendments: Approval

The proposed amendments meet the criteria of ZLDR Art. 3.2.6 and 3.3.6.

Planning Commission Workshop and Meeting: October 12, 2020

Recommendation: Defer to the November meeting to consider right-of-way dedication requirements, incentives for dedicating rights-of-way, and increasing the base density requirements (vote: 6 to 0).

Notifications: On September 25, 2020, 1,434 Planning Commission meeting notifications were sent to individuals on the Johns Island, Johns Island Maybank Highway and ZLDR/Comprehensive Plan Interested Parties’ Lists, as well as owners of properties within the JO-MHC-O boundary and within 300 feet of it. Additionally, the Planning Commission meeting was noticed in the Post & Courier on September 25, 2020.

Planning Commission Workshop and Meeting: November 9, 2020

Recommendation: Approval with staff recommended changes (vote 9-0).

Staff recommendations included:

- Removal of “Warehouse Clubs or Superstores” as an allowed use in the Mixed-Use District by the request of the Johns Island Task Force;
- Revise density, intensity, and dimensional standards for the Mixed Use and Limited Commercial Districts to provide more incentives to dedicate rights-of-way for a future connector road system.

Speakers:

- John Zlogar, representing the Johns Island Task Force, spoke in support of the proposal. He emphasized how important it was to maintain the 75-foot buffer requirement in order to preserve the Town and Country concept intended by the Overlay Zoning District.
- Jim Bobo, a property owner on both Johns and James Islands, also spoke in support of the proposed Overlay Zoning District. He emphasized the importance of increasing densities in the Mixed Use and Limited Commercial districts in order to maintain concentration of higher intensity uses in these areas, thereby containing urban sprawl.
- Allen Knapp, representing the HOA for the Villages of St. Johns Woods, spoke in opposition of the proposed Overlay Zoning District. He expressed concerns that the Overlay Zoning District would allow for higher density buildings in close proximity to his neighborhood. He added that this area was not equipped to handle increased pressures from more traffic, and that newly permitted industrial buildings behind the residential area of St. Johns Woods would result in decreased property values for homeowners.

Notifications: On October 23, 2020, 1,434 Planning Commission meeting notifications were sent to individuals on the Johns Island, Johns Island Maybank Highway and ZLDR/Comprehensive Plan Interested Parties’ Lists, as well as owners of properties within the JO-MHC-O boundary and within 300 feet of it. Additionally, the Planning Commission meeting was noticed in the Post & Courier on October 23, 2020.

Public Hearing: January 19, 2021

Speakers:

- Linda Wood, Villages of St. Johns Woods
 - Access is via Stringfellow Ln.
 - Opposes Stringfellow Ln being designated as a future connector road.
- Rich Jenkins, represents the Johns Island Council
 - Supports the proposed overlay as it will help Johns Island move forward.
 - Likes the nodes.
- Pam Skinner, Wadmalaw Island
 - Supports the proposed overlay.
 - Also read a letter from Gregg Newby, Johns Island Community Association, which supports the proposed overlay including the connector road system.
- John Zlogar, Johns Island Task Force (JITF) Chair
 - Supports the proposed overlay district, which reinforces the town and country design concept for Johns Island.
 - Supports the interconnectivity/proposed connector road system.
 - Stated staff has listened to the JITF and addressed their concerns.
- Ryan McClure, Johns Island resident
 - Mixed Use nodes are important to allow density where appropriate.
 - Stated staff has done a wonderful job hosting community meetings and conveying the message.

Notifications: On December 31, 2020, 1,434 Public Hearing notifications were sent to individuals on the Johns Island, Johns Island Maybank Highway, and ZLDR/Comprehensive Plan Interested Parties' Lists, as well as owners of properties within the JO-MHC-O boundary and within 300 feet of it. Additionally, 24 signs were posted on Unincorporated Charleston County parcels on December 17, 2020 the public hearing was noticed in the Post & Courier on December 18, 2020.

A faint, light-colored map of Johns Island is visible in the background, showing the island's coastline and internal road network. The map is centered on the island and serves as a backdrop for the text.

JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR OVERLAY ZONING DISTRICT (JO-MHC-O)

Planning Commission Workshop: January 13, 2020

Planning Commission Workshop and Meeting: October 12, 2020

Planning Commission Workshop and Meeting: November 9, 2020

Public Hearing: January 19, 2021

PPW Committee: January 28, 2021

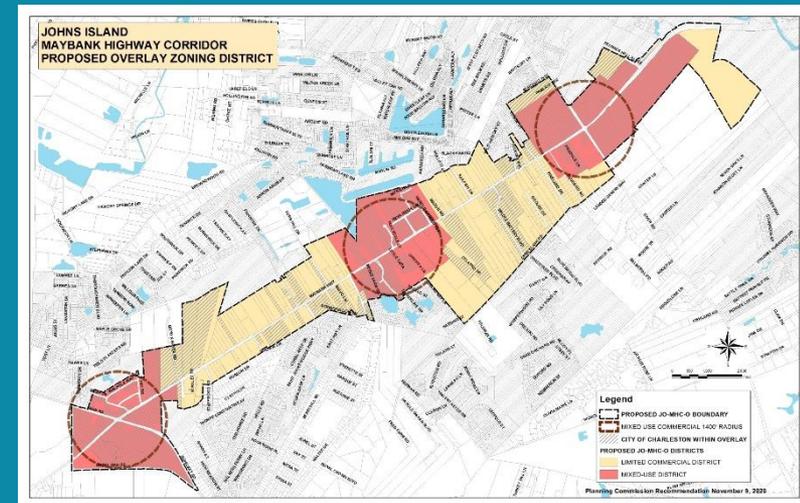
First Reading: February 2, 2021

Second Reading: February 16, 2021

Third Reading: March 9, 2021

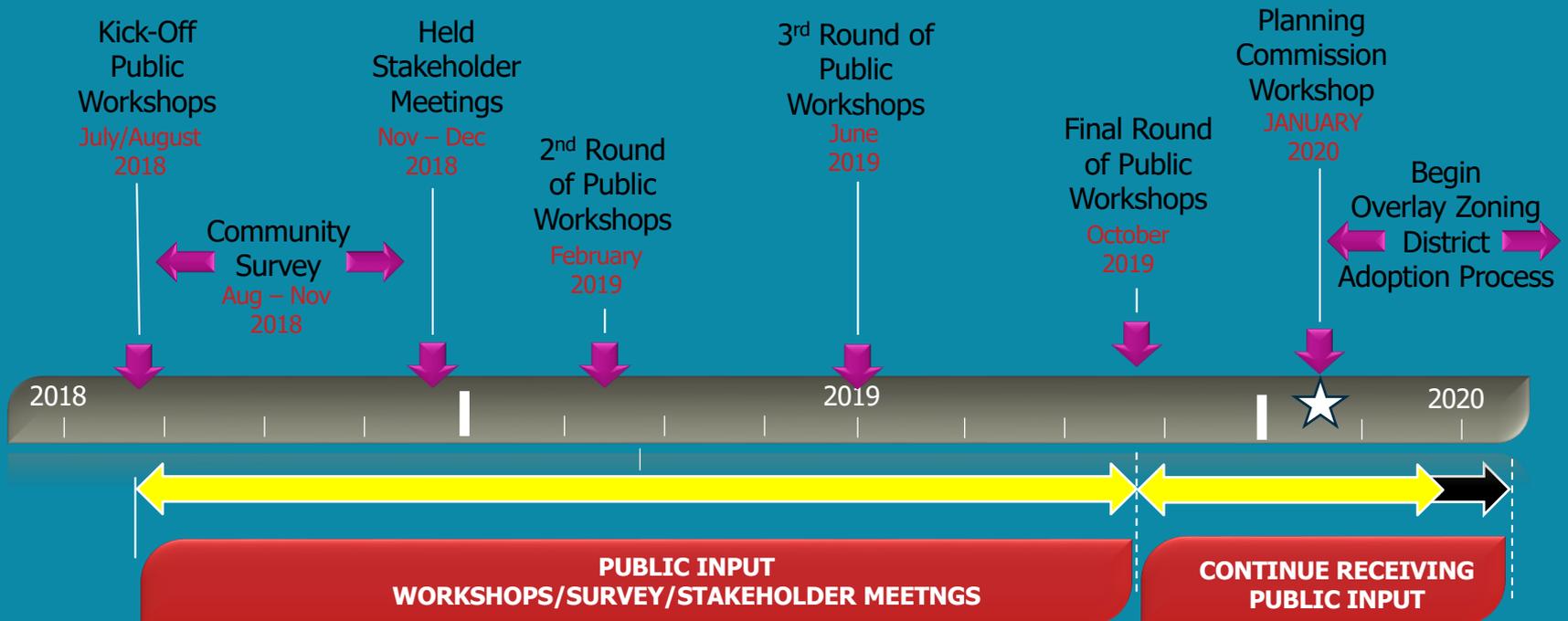
PROJECT HISTORY AND OVERVIEW

- **The JO-MHC-O District was first established as part of the 2001 ZLDR.**
 - 2006: The MHC-O was amended to implement the 2003 Comprehensive Plan Review.
 - 2018: The 10-Year Comprehensive Plan Update was adopted, including the priority recommendation to coordinate with the City of Charleston and residents to review and update the MHC-O.
- **The MHC-O creates two districts (the Mixed Use District and the Limited Commercial District) and:**
 - Implements consistent and coordinated land use, design, and development standards;
 - Addresses stormwater runoff, drainage, and flooding issues; and
 - Supports transportation and pedestrian safety improvements.

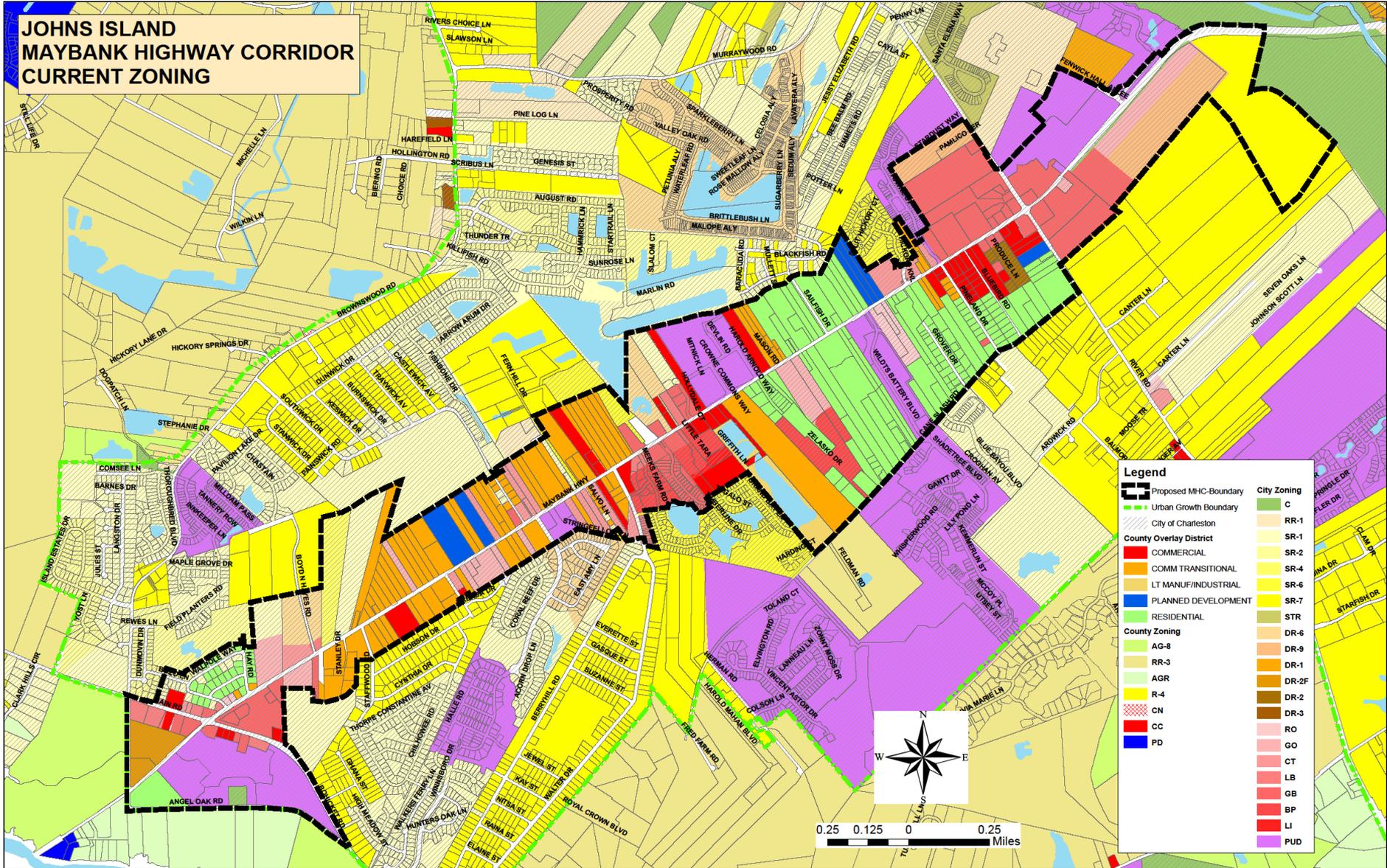


PROJECT OVERVIEW AND HISTORY

- Four Rounds of Public Workshops
- Two Community Surveys
- Three Stakeholder Meetings
- Numerous City-County Staff-level Meetings

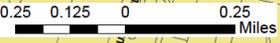


JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR CURRENT ZONING

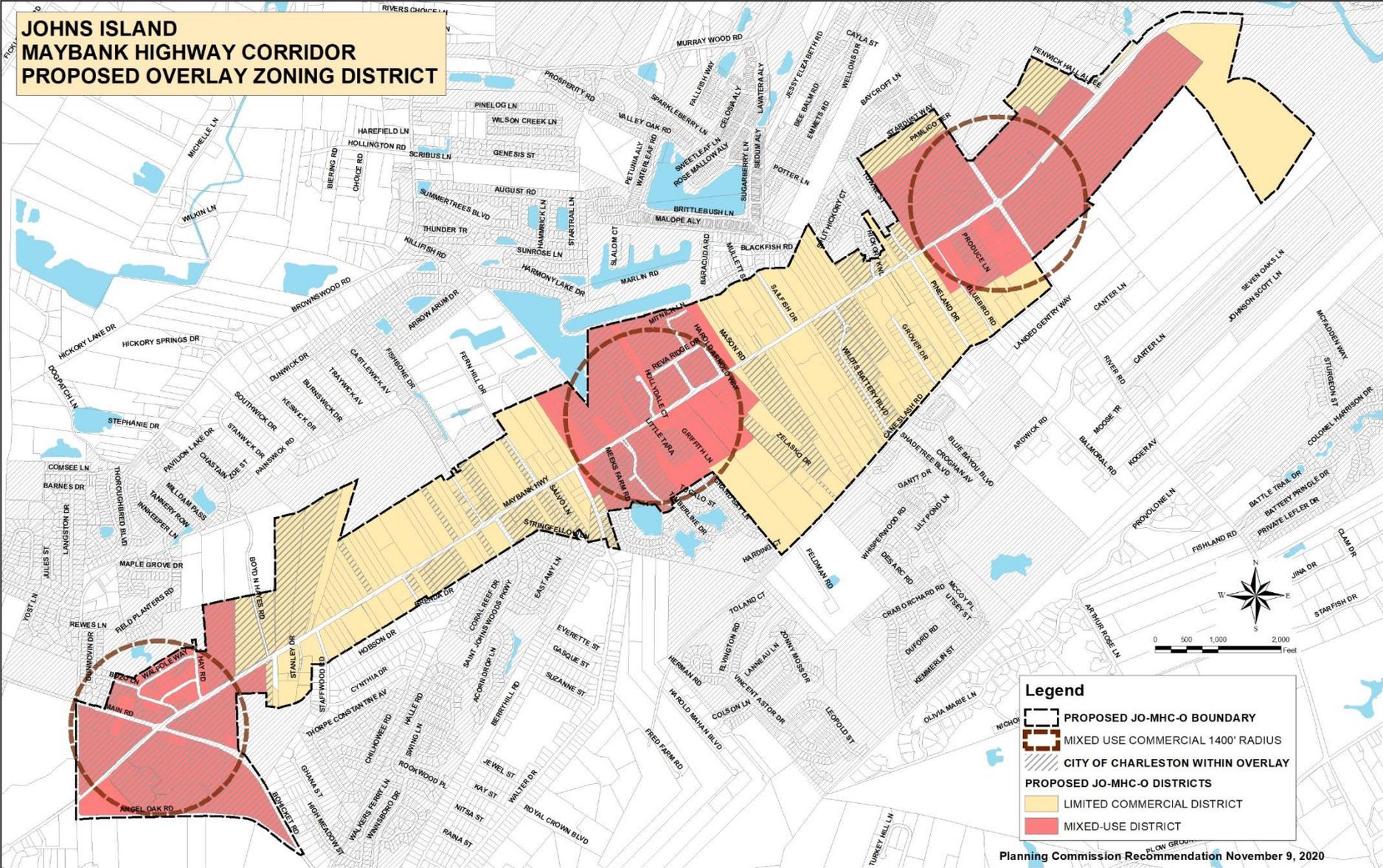


Legend

- Proposed MHC-Boundary
- Urban Growth Boundary
- City of Charleston
- County Overlay District**
- COMMERCIAL
- COMM TRANSITIONAL
- LT MANUF/INDUSTRIAL
- PLANNED DEVELOPMENT
- RESIDENTIAL
- City Zoning**
- C
- RR-1
- SR-1
- SR-2
- SR-4
- SR-6
- SR-7
- STR
- DR-6
- DR-9
- DR-1
- DR-2F
- DR-2
- DR-3
- RO
- GO
- CT
- LB
- GB
- BP
- LI
- PUD



JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR PROPOSED OVERLAY ZONING DISTRICT



Legend

- PROPOSED JO-MHC-O BOUNDARY
- MIXED USE COMMERCIAL 1400' RADIUS
- CITY OF CHARLESTON WITHIN OVERLAY
- PROPOSED JO-MHC-O DISTRICTS**
- LIMITED COMMERCIAL DISTRICT
- MIXED-USE DISTRICT

Planning Commission Recommendation November 9, 2020

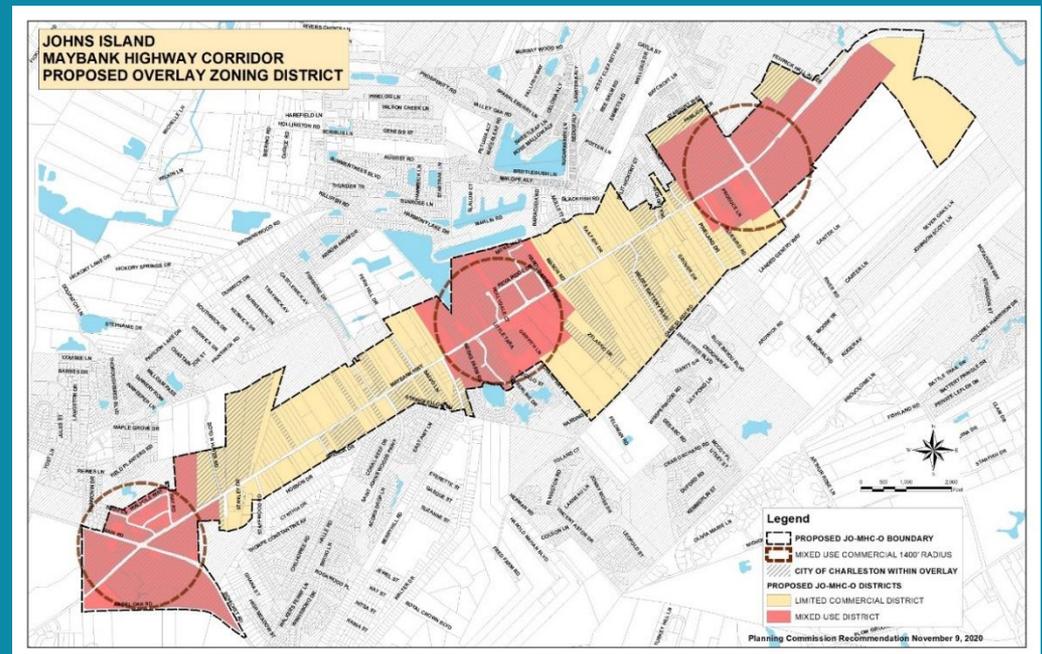
COMPREHENSIVE PLAN AMENDMENTS

- **Amend Chapter 3.1 as follows:**

- ✓ Update Map 3.1.4: Future Land Use to amend the JO-MHC-O;
- ✓ Update Map 3.1.5: Urban/Suburban Future Land Use Detail to amend the JO-MHC-O;
- ✓ Update Map 3.1.8: Johns Island Maybank Highway Corridor Overlay Zoning District;
- ✓ Add Map 3.1.19: Johns Island Maybank Highway Corridor Infrastructure and Connectivity Map.

- **Amend Chapter 3.9 as follows:**

- ✓ In Table 3.9.1, update the Project Status column to show “Completed in 2020”;
- ✓ Remove the Maybank Highway Corridor Overlay Zoning District project from Table 3.9.2.



APPROVAL CRITERIA – SECTION 3.2.6

Pursuant to Article 3.2.6 of the Charleston County Zoning and Land Development Regulations Ordinance, Comprehensive Plan Amendments may be approved by the County Council only if they determine that the proposed amendment is consistent with the overall purpose and intent of the Comprehensive Plan and that any one of the following criteria has been met:

- A. There was a significant error in the original Comprehensive Plan adoption; Response: N/A
- B. In adopting the Comprehensive Plan, the County Council failed to take into account facts, projections or trends that were reasonably foreseeable to exist in the future; Response: N/A
- C. Events, trends, or facts after adoption of the Comprehensive Plan have changed the County Council's original findings made upon plan adoption; Response: N/A

APPROVAL CRITERIA – SECTION 3.2.6

D. Events, trends, or facts after adoption of the Comprehensive Plan have changed the character or condition of an area, making the proposed amendment necessary;

Response: The Maybank Highway Corridor Overlay Zoning District was developed in coordination with the residents of Johns Island and the City of Charleston in the late 1980s/early 1990s. Since that time, many changes affecting this corridor have taken place, including:

- ***The City of Charleston revised their Maybank Highway Corridor Overlay Zoning District, adopting land use, density, and site design standards that differ from those adopted by Charleston County; and***
- ***Charleston County Council approved the construction of significant transportation improvements in the area, including widening Maybank Highway and incorporating bicycle and pedestrian ways, creating a pitchfork road design network north and south of Maybank Highway on the eastern side of River Road to manage traffic coming onto the island from James Island, and the final leg of Interstate 526 from West Ashley, across Johns Island, and onto James Island.***

These amendments implement the Comprehensive Plan strategy to review this Overlay Zoning District in light of the changes described above and work with the public and the City of Charleston to make revisions as appropriate.

E. The proposed Comprehensive Plan Amendment is requested pursuant to and complies with Art. 3.17, Developments of County Significance; Response: N/A

or

F. The proposed Comprehensive Plan Amendment is consistent with the comprehensive plan future land use recommendations of adjacent municipalities that have adopted extra-territorial jurisdiction for the subject parcel(s). Response: N/A

RECOMMENDATION

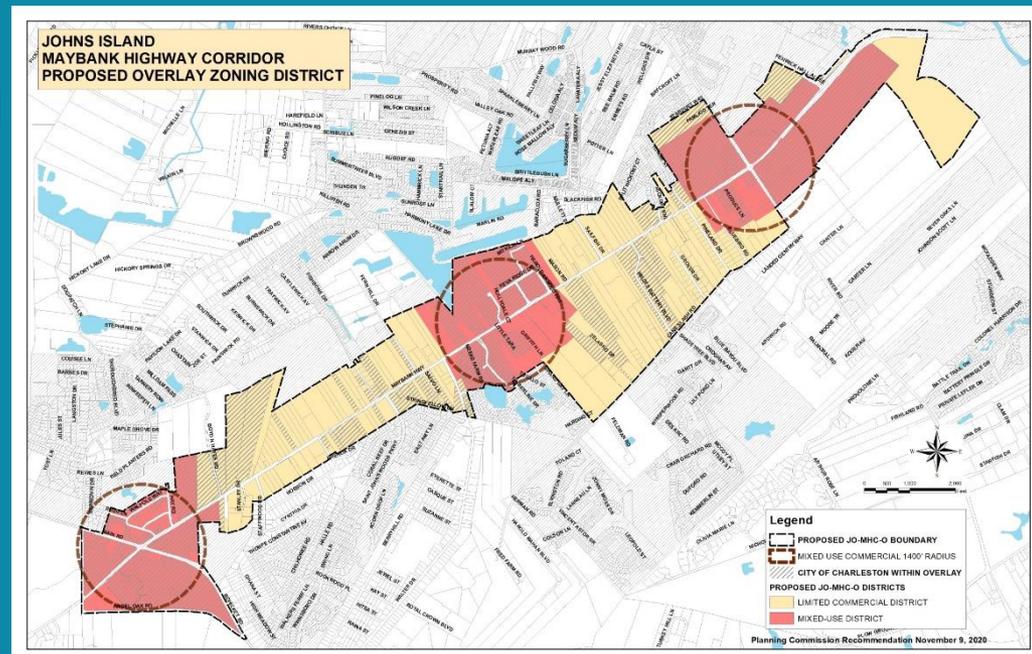
The Comprehensive Plan amendments meet the approval criteria.

STAFF AND PLANNING COMMISSION RECOMMENDATION:

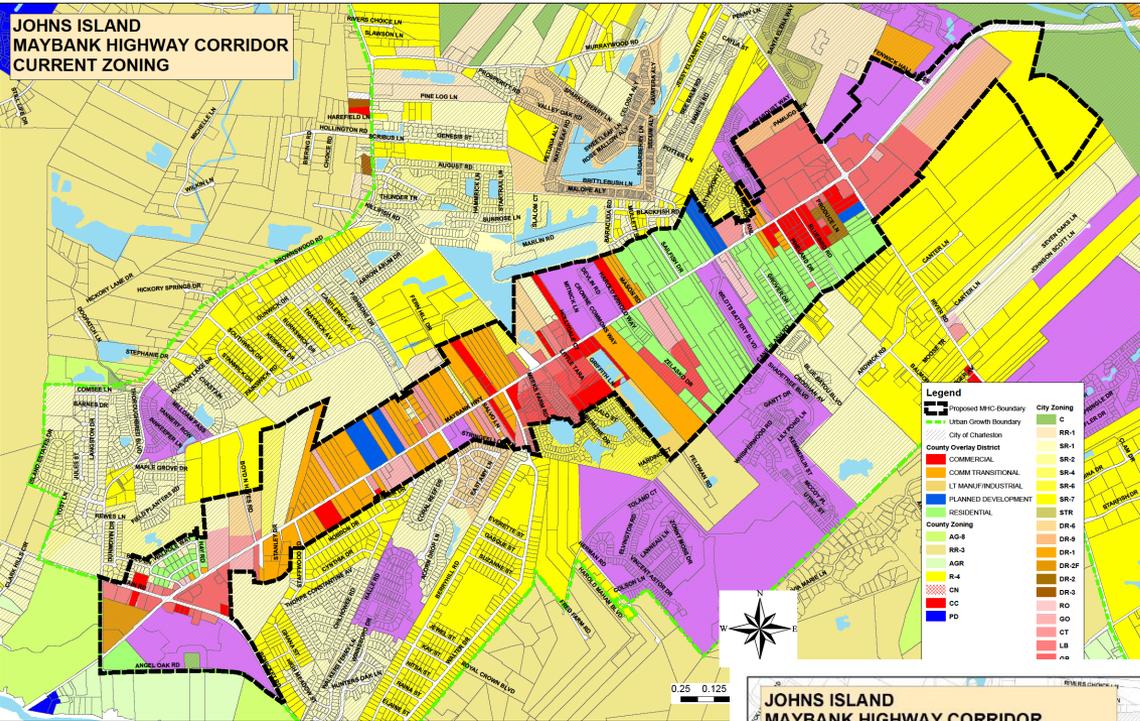
**APPROVAL OF THE
COMPREHENSIVE PLAN AMENDMENTS
Vote: 9 to 0**

ZLDR AMENDMENTS: JO-MHC-O

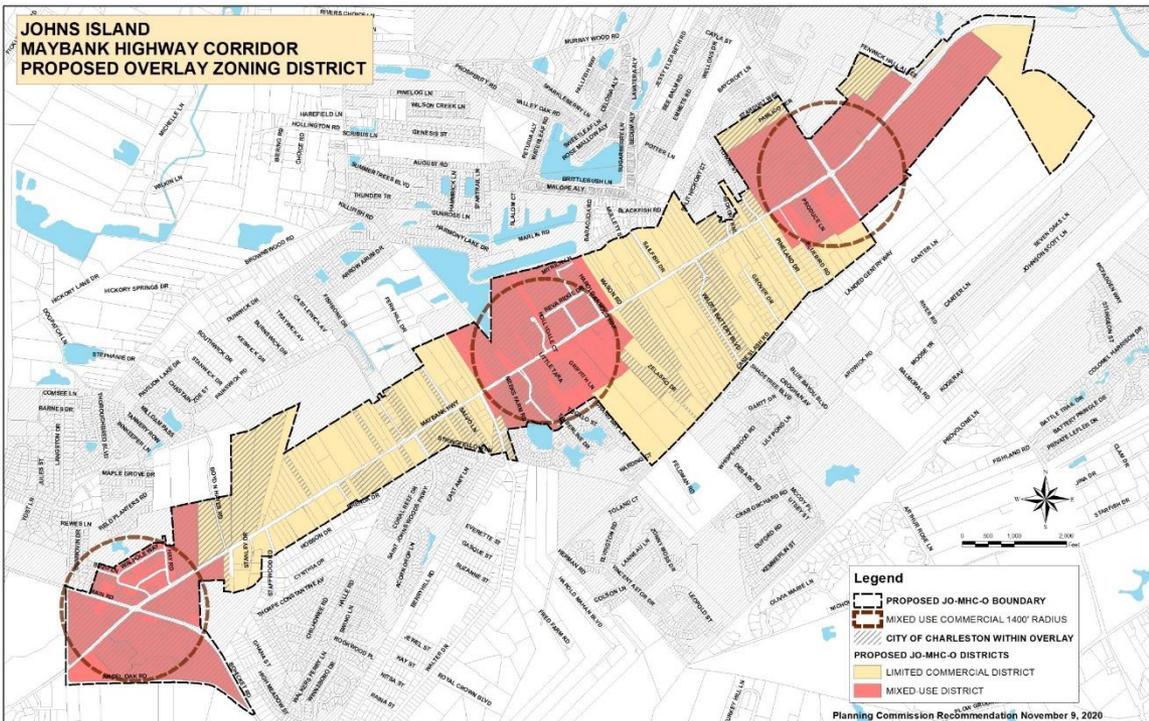
- The JO-MHC-O amends Article 5.4, MHC-O, Maybank Highway Corridor Overlay, to implement the Comprehensive Plan.
- It divides the corridor into two districts:
 - Mixed Use District: Intended for higher intensity commercial development at the three major nodes along the corridor; and
 - Limited Commercial District: Intended for service and neighborhood commercial uses.
- The JO-MHC-O also:
 - Implements consistent and coordinated land use, design, and development standards;
 - Addresses stormwater runoff, drainage, and flooding issues; and
 - Supports transportation and pedestrian safety improvements.



**JOHNS ISLAND
MAYBANK HIGHWAY CORRIDOR
CURRENT ZONING**



**JOHNS ISLAND
MAYBANK HIGHWAY CORRIDOR
PROPOSED OVERLAY ZONING**



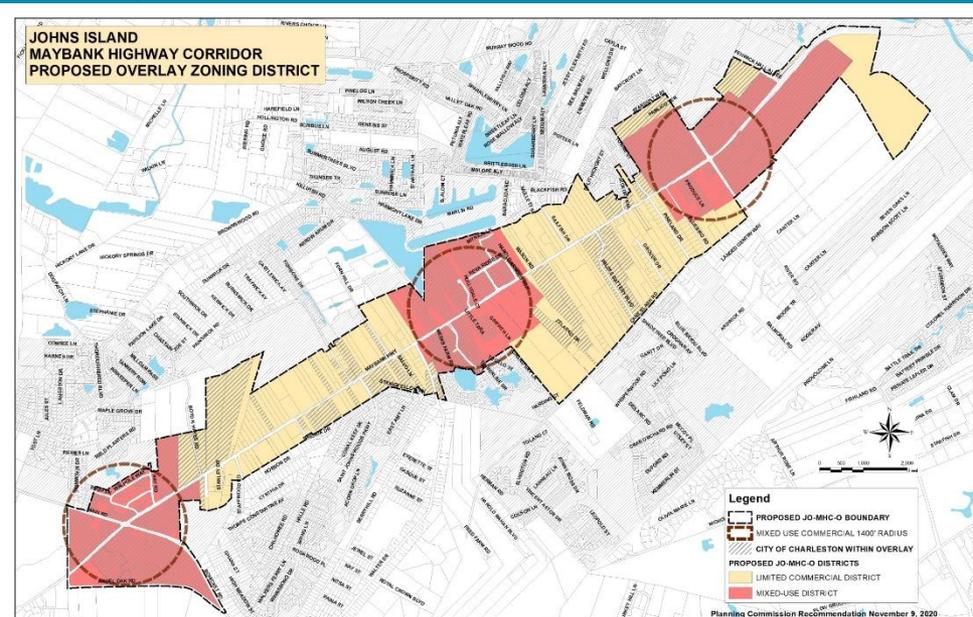
Limited Commercial District

- ▶ Farmers markets, crop production, agricultural processing and sales
- ▶ Single-family, duplexes, townhomes, tri/fourplexes
- ▶ Civic/institutional
- ▶ Offices and health care services
- ▶ Restaurants and bars
- ▶ Repair and maintenance services
- ▶ Retail sales
- ▶ Personal services
- ▶ Limited manufacturing and production
- ▶ Special trade contractors

Mixed Use District

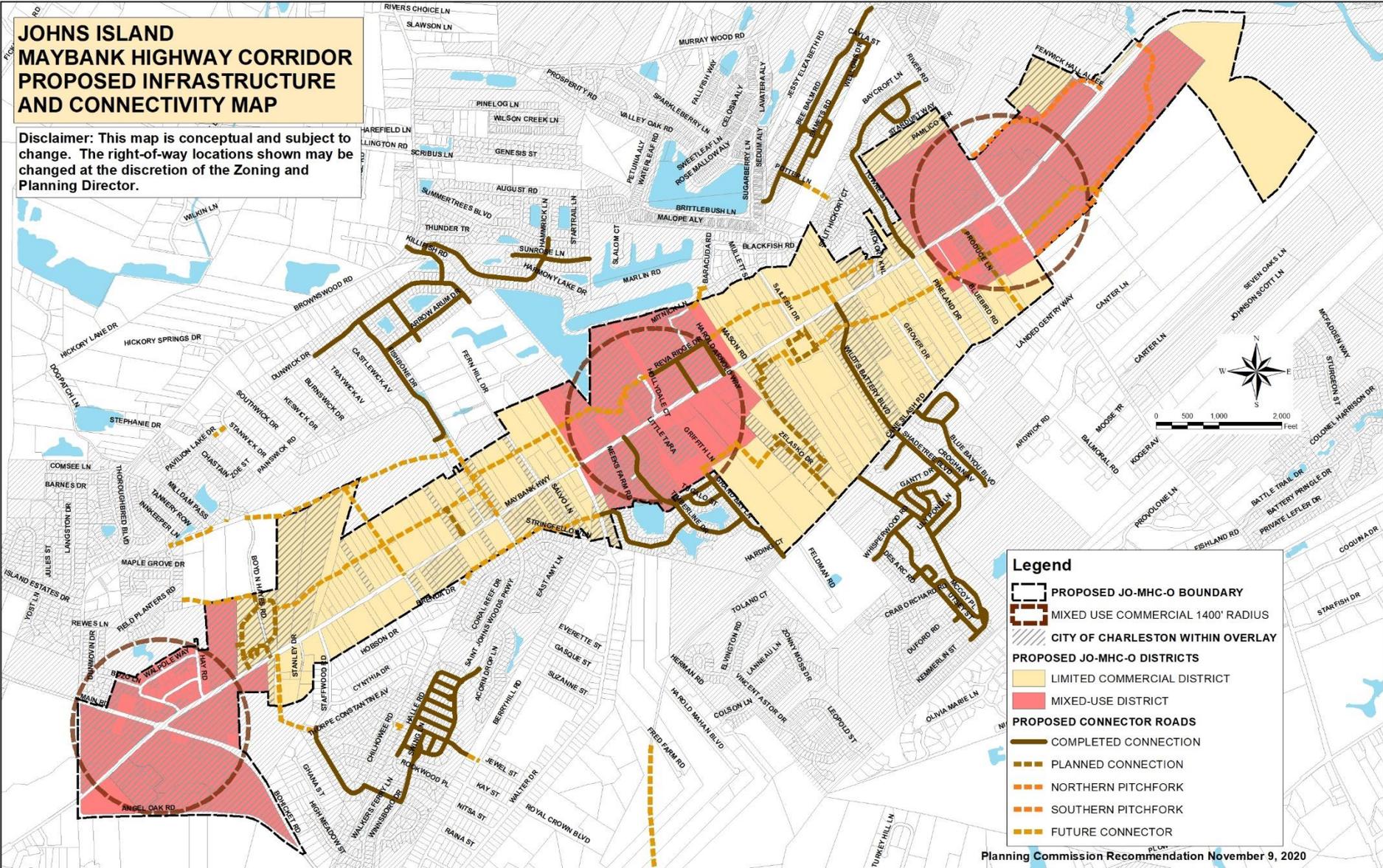
- ▶ Farmers markets, crop production, agricultural processing and sales
- ▶ Single and **multi-family**, duplexes, townhomes, tri/fourplexes
- ▶ Civic/institutional
- ▶ Offices and health care services
- ▶ Restaurants and bars
- ▶ Repair and maintenance services
- ▶ Retail sales
- ▶ **Home improvement centers, gas stations**
- ▶ Personal services
- ▶ **Wholesale sales**
- ▶ Limited manufacturing and production
- ▶ Special trade contractors
- ▶ **Parking garages**

LAND USES



JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR PROPOSED INFRASTRUCTURE AND CONNECTIVITY MAP

Disclaimer: This map is conceptual and subject to change. The right-of-way locations shown may be changed at the discretion of the Zoning and Planning Director.



Legend

- PROPOSED JO-MHC-O BOUNDARY
- MIXED USE COMMERCIAL 1400' RADIUS
- CITY OF CHARLESTON WITHIN OVERLAY
- PROPOSED JO-MHC-O DISTRICTS
- LIMITED COMMERCIAL DISTRICT
- MIXED-USE DISTRICT
- PROPOSED CONNECTOR ROADS
- COMPLETED CONNECTION
- PLANNED CONNECTION
- NORTHERN PITCHFORK
- SOUTHERN PITCHFORK
- FUTURE CONNECTOR

PEDESTRIAN/BICYCLE ACCESS

10' wide mixed use path required in the Limited Commercial District

10' wide sidewalk required in the Mixed Use District

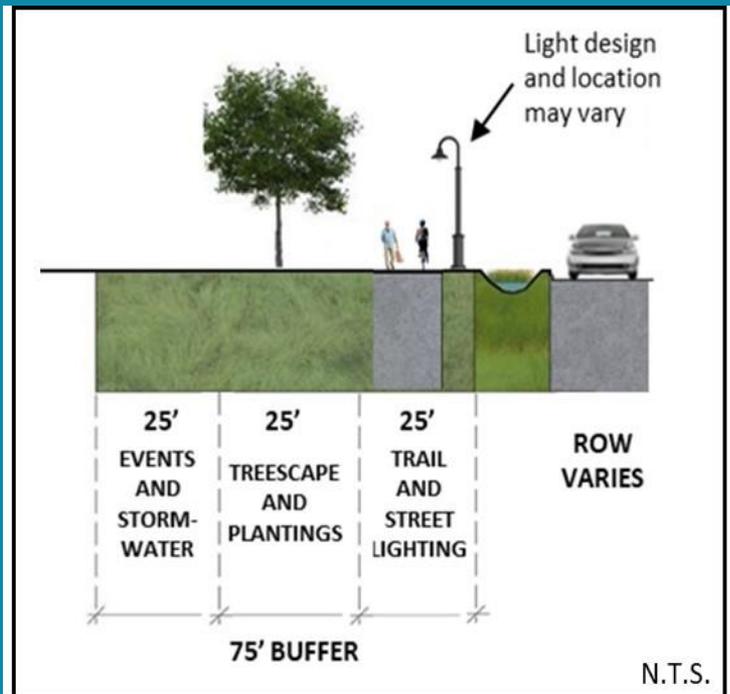


FIGURE 2
RIGHT-OF-WAY BUFFER, MAYBANK HIGHWAY

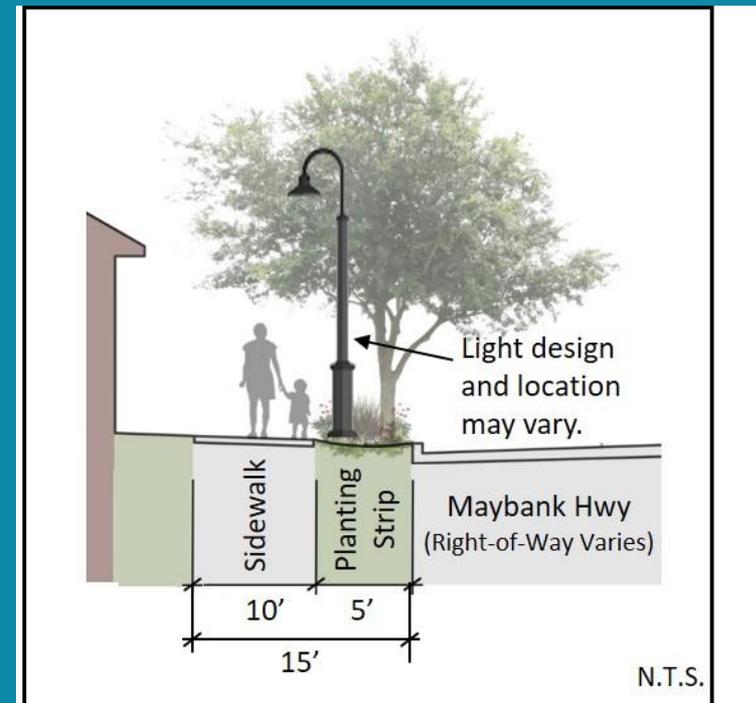


FIGURE 1
RIGHT-OF-WAY BUFFER, MAYBANK HIGHWAY

RIGHT-OF-WAY BUFFER REQUIREMENTS

Table 5.4-4, Limited Commercial (LC) District ROW Buffer Depth and Planting Schedule	
STANDARD	MAYBANK HIGHWAY (JOHNS ISLAND)
MIN. BUFFER DEPTH	75 feet
MINIMUM BUFFER LANDSCAPING (Plants per 100 linear feet)	
Canopy Trees – Must be Live Oak Species	3
Understory Trees (at least 50 percent evergreen]	4
Shrubs	30
Street Trees (may be counted toward canopy tree req.)	2

Table 5.4-2, Mixed-Use (MU) District Buffer Depth and Planting Schedule	
STANDARD	MAYBANK HIGHWAY (JOHNS ISLAND)
MIN. BUFFER DEPTH	15 feet
MINIMUM BUFFER LANDSCAPING (Plants per 100 linear feet)	
Canopy Trees – Must be Live Oak Species	3
Understory Trees (at least 50 percent evergreen]	3
Shrubs	25
Street Trees (may be counted toward canopy tree req.)	2

DEVELOPMENT STANDARDS

- Monument style signs:
 - Maximum height: 10'
 - Maximum size: 40 SF
 - Shared signs: Maximum height of 12' and maximum size of 60 SF
- 17' tall street lights required.
- Limits density and lot area calculations to highland areas only (does not include OCRM Critical Line area or freshwater wetlands).
- Right-in/right-out only required for driveway separations of less than 150'.
- All construction activities may be subject to Special Protection Area stormwater design criteria.

MU DISTRICT: DEVELOPMENT STANDARDS

STANDARD	WITHOUT 50' RIGHT-OF-WAY DEDICATION (No vertical improvements in connector road area)	WITH 50' RIGHT-OF-WAY DEDICATION
MAXIMUM RESIDENTIAL DENSITY	8 dwelling units per acre	19 dwelling units per acre
MINIMUM LOT WIDTH	12 feet	12 feet
MINIMUM SETBACKS	Equivalent to required buffers	Equivalent to required buffers
MAYBANK HIGHWAY RIGHT-OF-WAY BUFFER	15 feet	15 feet
OCRM CRITICAL LINE BUFFER	50 feet	50 feet
MAXIMUM BUILDING COVER	30% (40% with shared access)	50% (60% with shared access)
MAXIMUM INDIVIDUAL BUILDING FOOTPRINT	15,000 square feet	20,000 square feet (can get up to 50,000 SF via Special Exception)
MAXIMUM BUILDING HEIGHT	35 feet/2½ stories	55 feet/5 stories (portion above 35' must be stepped back from ROW)

LC DISTRICT: DEVELOPMENT STANDARDS

STANDARD	WITHOUT 50' RIGHT-OF-WAY DEDICATION (No vertical improvements in connector road area)	WITH 50' RIGHT-OF-WAY DEDICATION
MAXIMUM RESIDENTIAL DENSITY	6 dwelling units per acre	12 dwelling units per acre
MINIMUM LOT WIDTH	12 feet	12 feet
MINIMUM SETBACKS	Equivalent to required buffers	Equivalent to required buffers
MAYBANK HIGHWAY RIGHT-OF-WAY BUFFER	75 feet	75 feet
OCRM CRITICAL LINE BUFFER	50 feet	50 feet
MAXIMUM BUILDING COVER	30% (40% with shared access)	50% (60% with shared access)
MAXIMUM INDIVIDUAL BUILDING FOOTPRINT	5,000 square feet	10,000 square feet (can get up to 20,000 SF via Special Exception)
MAXIMUM BUILDING HEIGHT	35 feet/2½ stories	45 feet/3½ stories

CONNECTOR ROAD R-O-W EXAMPLE



ASSOCIATED AMENDMENTS TO OTHER SECTIONS OF THE ZLDR

- **ZLDR Sec. 8.4.2.A.4.j, Preliminary Plat Application Requirements:**

To clarify that a United States Army Corps of Engineers (USACE) jurisdictional determination is required for approval of a subdivision plat application as required for the Johns Island Maybank Highway Corridor Overlay Zoning District.

- **ZLDR Sec. 8.5.2.B.9, Final Plat Application Requirements:**

To clarify that a United States Army Corps of Engineers (USACE) jurisdictional determination is required for approval of a subdivision plat application as required for the Johns Island Maybank Highway Corridor Overlay Zoning District.

- **ZLDR Sec. 9.5.4, Table 9.5.4.A.3, Buffer Types by Roadway:**

Amend the table to change “Maybank Highway Corridor Overlay Zoning District” to “Maybank Highway Corridor Overlay Zoning District (Johns Island), and change footnote 2 to state: “Buffer type shall be as described in the Johns Island Maybank Highway Corridor Overlay Zoning District.”

APPROVAL CRITERIA – SECTION 3.3.6

Pursuant to Article 3.3.6 of the Charleston County Zoning and Land Development Regulations Ordinance, text amendments to the ordinance may be approved if the following approval criteria have been met:

- A. The proposed amendment corrects an error or inconsistency or meets the challenge of a changing condition.**

Response: The proposed overlay zoning district meets the challenge of the changing conditions of the Johns Island Maybank Highway corridor by improving pedestrian mobility, requiring standards for safer and more efficient vehicle access, enhancing right-of-way buffer and signage requirements, and adding more restrictive density/intensity and dimensional standards.

- B. The proposed amendment is consistent with the adopted Charleston County Comprehensive Plan and goals as stated in Article 1.5;**

Response: The proposed overlay zoning district is consistent with the adopted Charleston County Comprehensive Plan and goals as stated in Article 1.5 as it implements the Comprehensive Plan's priority recommendation to review and revise the Johns Island Maybank Highway Corridor Overlay Zoning District.

- C. The proposed amendment is to further the public welfare in any other regard specified by County Council;**

Response: The proposed overlay zoning district furthers the public welfare by: improving the safety and efficiency of vehicle, pedestrian, and bicycle in the corridor; creating consistency in permitted land uses; and addressing drainage issues through Special Protection Area stormwater design criteria.

RECOMMENDATIONS

The ZLDR text amendments meet the approval criteria.

STAFF AND PLANNING COMMISSION RECOMMENDATION:

**APPROVAL OF AMENDMENTS TO ART. 5.4, JO-
MHC-O (CURRENTLY MHC-O), JOHNS ISLAND
MAYBANK HIGHWAY
CORRIDOR
OVERLAY ZONING DISTRICT**

Vote: 9 to 0

NOTIFICATIONS

■ **Planning Commission Meeting Notifications:**

- **September 25, 2020 (for the Oct. 12, 2020 Planning Commission Workshop & Meeting)**
 - 1,434 Planning Commission meeting notifications were sent to individuals on the County's Johns Island, Johns Island Maybank Highway and Main Road, and ZLDR/Comprehensive Plan Interested Parties' Lists, as well as owners of properties within the JO-MHC-O boundary and within 300 feet of it.
 - Ad ran in the *Post & Courier*.

- **October 23, 2020 (for the Nov. 9, 2020 Planning Commission Workshop & Meeting)**
 - 1,434 Planning Commission meeting notifications were sent to individuals on the County's Johns Island, Johns Island Maybank Highway and Main Road, and ZLDR/Comprehensive Plan Interested Parties' Lists, as well as owners of properties within the JO-MHC-O boundary and within 300 feet of it.
 - Ad ran in the *Post & Courier*.

■ **Public Hearing Notifications:**

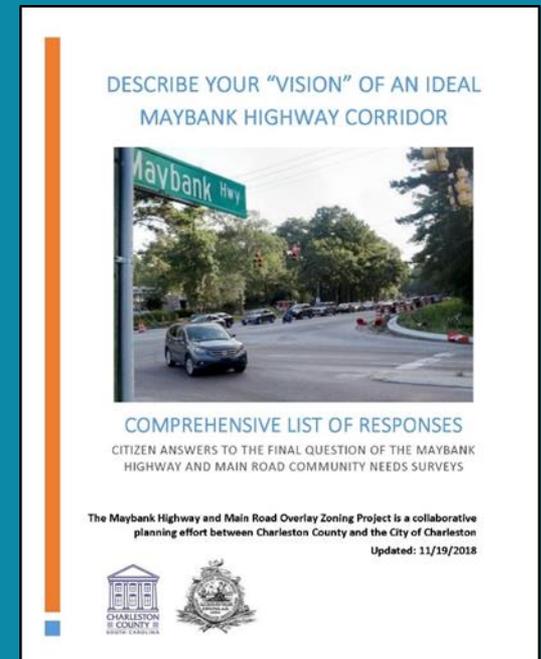
- **December 17/18, 2020 (for the Jan. 19, 2021 Public Hearing)**
 - Ad ran in the *Post & Courier* and 24 signs posted on Unincorporated Charleston County parcels along the corridor.

- **December 31, 2020 (for the Jan. 19, 2021 Public Hearing)**
 - 1,434 notifications were sent to individuals on the County's Johns Island, Johns Island Maybank Highway, and Main Road, and ZLDR/Comprehensive Plan Interested Parties' Lists, as well as owners of properties within the JO-MHC-O boundary and within 300 feet of it.

SUMMARY OF THE PUBLIC INPUT RECEIVED FROM COMMUNITY SURVEY

As part of the initial public input process, a Johns Island Community Survey was conducted during the month of August 2018. The survey was specifically tailored to the Maybank Highway and Main Road corridors on Johns Island, and was made available in paper at each of the community meetings, at the Johns Island public library, and online on the Charleston County Planning Department's Maybank Highway and Main Road Overlay Zoning Project webpage. A total of 1,725 Johns Island Community Surveys were completed, with the results showing the following community concerns:

- Widening Maybank Highway to four lanes;
- Improved traffic and road conditions;
- Preserve the Rural Area and natural resources;
- More commercial ventures so residents are not required to leave the island;
- Improved infrastructure to address drainage and flooding threats;
- Address transportation issues, including alternative modes of transportation (bike paths, pedestrian ways, etc.), better connectivity of existing road networks, and evacuation routes; and
- Slowed residential development, at least until infrastructure improvements are made.



PUBLIC INPUT

- **Prior to PC Recommendation**
 - Three letters received:
 - One had questions regarding the location of the proposed connector road in relation to her property.
 - One requested that the 75' buffer be reduced to 25-35'.
 - One stated the plan allows too much development and not enough conservation.
- **Public Comment at Nov. 9 Planning Commission**
 - Two speakers spoke in support (John Zlogar – Johns Island Task Force and Jim Bobo), emphasizing importance of 75' buffer and increased density in the MU District.
 - One speaker in opposition representing Villages of St. Johns Lakes; concerns about increased density near the neighborhood, traffic, and decreased property value.
- **Letters Received Prior to Jan. 19 Public Hearing**
 - 7 letters received regarding the proposed connectivity road running through Stringfellow Lane in the Villages of St. Johns Woods.
 - One letter regarding the need for flooding to be addressed near Produce Lane.
 - One letter requesting more information about the connector road system in Barberry Woods subdivision.

JAN. 19 PUBLIC HEARING COMMENTS

<p>Linda Wood <i>Villages of St. Johns Woods</i></p>	<ul style="list-style-type: none">• Access is via Stringfellow Ln.• Opposes Stringfellow Ln being designated as a future connector road.
<p>Rich Jenkins <i>Johns Island Council</i></p>	<ul style="list-style-type: none">• Supports the proposed overlay as it will help Johns Island move forward.• Likes the nodes.
<p>Pam Skinner <i>Wadmalaw Is. resident</i></p>	<ul style="list-style-type: none">• Supports the proposed overlay.• Also read a letter from Gregg Newby, Johns Island Community Association, which supports the proposed overlay including the connector road system.
<p>John Zlogar <i>Johns Island Task Force (JITF) Chair</i></p>	<ul style="list-style-type: none">• Supports the proposed overlay district which reinforces the town and country design concept for Johns Island.• Supports the interconnectivity/proposed connector road system.• Stated staff has listened to the JITF and addressed their concerns.
<p>Ryan McClure <i>Johns Island resident</i></p>	<ul style="list-style-type: none">• Mixed use nodes are important to allow density where appropriate.• Stated staff has done a wonderful job hosting community meetings and conveying the message.

A faint, light-colored map of Johns Island, South Carolina, is visible in the background of the slide. The map shows the island's coastline, major roads, and water bodies.

JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR OVERLAY ZONING DISTRICT (JO-MHC-O)

Planning Commission Workshop: January 13, 2020

Planning Commission Workshop and Meeting: October 12, 2020

Planning Commission Workshop and Meeting: November 9, 2020

Public Hearing: January 19, 2021

PPW Committee: January 28, 2021

First Reading: February 2, 2021

Second Reading: February 16, 2021

Third Reading: March 9, 2021

**Draft Article 5.4:
Proposed Johns Island
Maybank Highway
Corridor Overlay Zoning
District ZLDR
Amendments**

ARTICLE 5.4 JO-MHC-O, JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR OVERLAY ZONING DISTRICT

§ 5.4.1 Statement of Findings

The MHC-O, Maybank Highway Corridor Overlay Zoning District, was first established as part of the Charleston County Zoning and Land Development Regulations Ordinance (ZLDR) when it was adopted on November 20, 2001. The MHC-O only included unincorporated properties along Maybank Highway located on Johns Island. In 2006, the MHC-O district was amended to implement the recommendations of the 2003 *Charleston County Comprehensive Plan Five-Year Review*, but still only included unincorporated properties along Maybank Highway on Johns Island. In 2018, the *10-Year Comprehensive Plan Update* was adopted, and includes the priority recommendation to coordinate with the City of Charleston to review the MHC-O District and extend the overlay boundary onto James Island. The JO-MHC-O, Johns Island Maybank Highway Corridor Overlay Zoning District, implements that recommendation.

This Article replaces the original MHC-O District, and is renamed as JO-MHC-O, Johns Island Maybank Highway Corridor Overlay Zoning District. The corridor is a primary gateway to Johns Island and acts as one of the main thoroughfares for destinations such as Kiawah Island, Seabrook Island, and Wadmalaw Island. Maybank Highway on Johns Island carries a high volume of vehicular traffic each day. Moreover, some of the properties within this corridor are located within the jurisdictional limits of the City of Charleston, while some are located within unincorporated Charleston County. The revised JO-MHC-O District was adopted to implement traffic safety measures, address infrastructure deficiencies, improve the visual character of the corridor, and create consistency between the County of Charleston and the City of Charleston concerning land development regulations.

§ 5.4.2 Purpose and Intent

The purpose of the JO-MHC-O District is to create a corridor that is well-planned, attractive, and preserves and improves existing development patterns where appropriate through the implementation of traffic safety measures and land use and design standards. It is also intended to be consistent with similar regulations adopted by the City of Charleston, ensure safe and efficient vehicle, pedestrian, and bicycle movement through traffic safety measures and access management standards, and to address stormwater runoff, drainage, and flooding issues along the corridor.

A separate regional planning effort involving numerous public and private stakeholders was conducted concurrent with the preparation of the JO-MHC-O District. This planning effort, named Dutch Dialogues Charleston, culminated in a final report released in September 2019. The report addresses “long-term planning to manage the risks and the opportunities provided by the Lowcountry’s dynamic water systems.” It should be noted that implementation of recommendations included in the Dutch Dialogues Charleston Final Report may result in future amendments to the JO-MHC-O District.

§ 5.4.3 Applicability and Effect of the Overlay Zoning District

A. Applicability

The standards of this Article shall apply to all development of unincorporated properties within the JO-MHC-O District, as shown on the map titled “Johns Island Maybank Highway Corridor Overlay Zoning District,” except Single-Family Detached Dwellings.

B. Effect of Overlay Zoning District

In case of conflict between the regulations of this Article and other regulations in this Ordinance, the regulations of this Article shall control.

C. Variances

The following requirements are ineligible for Zoning Variances in addition to those listed in Sec. 3.10.1, Applicability; Limitations, of this Ordinance:

1. Sec. 5.4.7.B, Vehicle Access;
2. Sec. 5.4.7.D, Pedestrian Access and Sidewalks/Multi-Use Path;
3. Sec. 5.4.8.A.2, MU District Right-of-Way Buffers; and
4. Sec. 5.4.9.A.2, LC District Right-of-Way Buffers.

§ 5.4.4 COORDINATION WITH ADJACENT JURISDICTIONS

A letter of coordination from the City of Charleston shall be required as part of all land development applications in the JO-MHC-O District with the exception of applications for Single-Family Detached Dwellings. The purpose of this coordination is to ensure that development is consistent with similar requirements adopted for the JO-MHC-O District by the City of Charleston.

§ 5.4.5 USE REGULATIONS

A. Table 5.4-1, Johns Island Maybank Highway Overlay Zoning District Use Table. Table 5.4-1 lists the Principal Uses permitted in the districts as identified and described in this Overlay Zoning District and as shown on the map entitled "Johns Island Maybank Highway Corridor Overlay Zoning District." The following is a description of the codes used in the table:

1. **Uses Allowed by Right.** "A" indicates uses allowed by right.
2. **Uses Subject to Conditions.** "C" indicates uses subject to conditions. A cross-reference to the applicable conditions can be found in the column entitled "Conditions."
3. **Uses Subject to Special Exception.** "S" indicates uses allowed only if reviewed and approved in accordance with the Special Exception procedures of this Ordinance, subject to compliance with use-specific conditions. A cross-reference to the applicable conditions can be found in the column entitled "Conditions."
4. **Prohibited Uses.** Blank cells indicate uses that are not permitted.

B. New or Unlisted Uses and Interpretation. The Zoning and Planning Director shall be authorized to make use determinations whenever there is a question regarding the category of use based on the definitions contained in Chapter 12, Definitions, of this Ordinance.

C. Nonconforming Uses. The requirements of Article 10.2, Nonconforming Uses, of this Ordinance apply to all development subject to the JO-MHC-O District, provided, however, that the following requirement shall apply in place of those contained in Sec. 10.2.4, Loss of Legal Nonconformity Status, sub-section A:

1. **Abandonment.** If a Nonconforming Use is replaced with another use or is discontinued for any reason for a period of more than 36 consecutive months, the use shall be considered abandoned. Once abandoned, the legal nonconforming status of the use shall be lost and re-establishment of a Nonconforming Use shall be prohibited. Any subsequent use of the property shall comply with the regulations of the JO-MHC-O District.

D. Accessory Uses. Accessory Uses shall be allowed pursuant to Art. 6.5, Accessory Uses, of this Ordinance.

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
 Planning Commission Recommendation November 9, 2020

Table 5.4-1, Johns Island Maybank Highway Overlay Zoning District Use Table			
Uses	Mixed-Use (MU) District	Limited Commercial (LC) District	Conditions
AGRICULTURAL			
AGRICULTURAL AND ANIMAL PRODUCTION, PROCESSING AND SUPPORT			
Aquaculture			
Mariculture			
Apiculture (Bee Keeping)			
Animal and Insect Production			
Concentrated Animal Feeding Operation			
Horticultural, Greenhouse, Nursery, Crop and Floriculture Production	A	A	
Hemp Crop Production and/or Processing	C	C	§5.4.6.A
Winery			
Agricultural Processing	A	A	
Agricultural Sale or Service	C	C	§5.4.6.B
Roadside Stand	A	A	
Farmers Market	A	A	
Sweetgrass Basket Stand	A	A	
Community Garden	A	A	
FORESTRY AND LOGGING			
Bona Fide Forestry Operation			
Lumber Mill, Planing, or Saw Mill			
RESIDENTIAL			
ASSISTED LIVING			
Assisted Living	A	A	
MANUFACTURED HOUSING			
Manufactured Housing Unit			
Manufactured Housing Park			
MULTI-FAMILY DWELLING			
Dwelling, Multi-Family	A		
Triplex and Fourplex	A	A	
Duplex	A	A	
Dwelling Group			
Dwelling, Single-Family, Attached	C	C	§5.4.6.C
SHORT-TERM RENTAL			
Short-Term Rental Property, Limited Home Rental (LHR)			
Short-Term Rental Property, Extended Home Rental (EHR)			

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
 Planning Commission Recommendation November 9, 2020

Table 5.4-1, Johns Island Maybank Highway Overlay Zoning District Use Table			
Uses	Mixed-Use (MU) District	Limited Commercial (LC) District	Conditions
SINGLE-FAMILY DWELLING			
Dwelling, Single-Family Detached	A	A	
OTHER RESIDENTIAL USES			
Transitional Housing	A	S	
Child Caring Institution			
Emergency Shelter	A	A	
Affordable Dwelling Unit	C	C	§5.4.6.E
Group Residential	A	A	
Farm Labor Housing			
CIVIC / INSTITUTIONAL			
COURTS AND PUBLIC SAFETY			
Court of Law	A	A	
Correctional Institution			
Parole Office or Probation Office			
Safety Service	A	A	
DAY CARE SERVICES			
Adult Day Care Service	A	A	
Family Home	C	C	§5.4.6.F
Group Home	A	A	
Child Care Center	A	A	
Day Camp	A	A	
DEATH CARE SERVICES			
Cemetery	A	A	
Funeral Service	A	A	
EDUCATIONAL SERVICES			
Pre-school or Educational Nursery	A	A	
School, Primary	A	A	
School, Secondary	A	A	
Higher Education Facility	A	A	
Personal Improvement Education	A	A	
HEALTH CARE SERVICES			
Medical Office or Outpatient Clinic	A	A	
Community Residential Care Facility	C	C	§5.4.6.G
Counseling Service	A	A	

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
 Planning Commission Recommendation November 9, 2020

Table 5.4-1, Johns Island Maybank Highway Overlay Zoning District Use Table			
Uses	Mixed-Use (MU) District	Limited Commercial (LC) District	Conditions
Intermediate Care Facility for Individuals with Intellectual Disabilities	A	A	
Health Care Laboratory	A	A	
Home Health Agency	A	A	
Hospital; Hospice Facility	A	A	
Outpatient Facility for Chemically Dependent or Addicted Persons	A	A	
Rehabilitation Facility	A	A	
Residential Treatment Facility for Children or Adolescents (mental health treatment)	A	A	
MUSEUM, HISTORIC SITE AND SIMILAR INSTITUTIONS			
Historic Site	A	A	
Library or Archives	A	A	
Museum	A	A	
Nature Exhibition	A		
Botanical Garden	A		
Zoo	S		
POSTAL SERVICE			
Postal Service, United States	A	A	
RECREATION AND ENTERTAINMENT			
Community Recreation	A	A	
Fishing, Hunting or Recreational Guide Service	A	A	
Golf Course or Country Club			
Parks and Recreation	A	A	
Recreation and Entertainment, Indoor	A	A	
Recreation and Entertainment, Outdoor	C	C	§5.4.6.H
Drive-In Theater			
Golf Driving Range			
Outdoor Shooting Range			
Special Event	C	C	§5.4.6.I
RELIGIOUS, CIVIC, PROFESSIONAL AND SIMILAR ORGANIZATIONS			
Business, Professional, Labor, Political Organizations; Social or Civic Organization; Social Club or Lodge	A	A	
Religious Assembly	A	A	
UTILITIES AND WASTE-RELATED USE			
Utility Service, Major	C	C	§5.4.6.J
Utility Service, Minor	A	A	

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
 Planning Commission Recommendation November 9, 2020

Table 5.4-1, Johns Island Maybank Highway Overlay Zoning District Use Table			
Uses	Mixed-Use (MU) District	Limited Commercial (LC) District	Conditions
Septic Tank Installation, Cleaning, or Related Services	A	A	
Solid Waste Disposal Facility (Public or Private)			
COMMERCIAL			
ACCOMMODATIONS			
Short-Term Rental Property, Commercial Guest House (CGH)	C	C	§5.4.6.D
Hotel or Motel	A		
RV (Recreational Vehicle) Park			
Campground			
ANIMAL SERVICES			
Stable, Commercial			
Stable, Private			
Kennel	A	A	
Pet Store or Grooming Salon	A	A	
Small Animal Boarding	A	A	
Veterinary Service	A	A	
FINANCIAL SERVICES			
Banks or Financial Services	A	A	
Short-term Lender			
FOOD SERVICES AND DRINKING PLACES			
Bar or Lounge	S	S	
Catering Service	A	A	
Restaurant, Fast Food	C	C	§5.4.6.K
Restaurant, General	C	C	§5.4.6.K
Sexually Oriented Business			
INFORMATION INDUSTRIES			
Communication Service; Data Processing Service; and Publishing Industry	A	A	
Communications Tower	C	C	§6.4.5
OFFICES			
Administrative or Business Office; Government Office; Professional Office	A	A	
OTHER NONRESIDENTIAL DEVELOPMENT			
Convention Center or Visitors Bureau	A	A	
Heavy Construction Service or General Contractor	C	C	§5.4.6.L
Billboard			
Special Trade Contractor (Office/Storage)	C	C	§5.4.6.L

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
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Table 5.4-1, Johns Island Maybank Highway Overlay Zoning District Use Table			
Uses	Mixed-Use (MU) District	Limited Commercial (LC) District	Conditions
PARKING, COMMERCIAL			
Parking Lot	A	A	
Parking Garage	A		
RENTAL AND LEASING SERVICES			
Charter Boat or other Recreational Watercraft Rental Service			
Commercial or Industrial Machinery or Equipment, Construction Tools or Equipment, Heavy Duty Truck or Commercial Vehicle Rental or Leasing	C	C	§5.4.6.M
Consumer Goods Rental Center	A	A	
Self-Service Storage Facility	C	C	§5.4.6.N
Vehicle Rental or Leasing	C	C	§5.4.6.M
REPAIR AND MAINTENANCE SERVICES			
Boat Yard	A	A	
Repair Service, Consumer	C	C	§5.4.6.L
Repair Service, Commercial	C	C	§5.4.6.L
Vehicle Repair or Service	A	C	§5.4.6.L
RETAIL SALES			
Nonstore Retailer	A	A	
Fuel Dealer; Heating Oil Dealer; Liquefied Petroleum Gas (Bottled Gas) Dealer	C	C	§5.4.6.O
Home Improvement Center	A		
Food Sales	A	A	
Food Truck	A	A	
Liquor, Beer, or Wine Sales	S	S	
Retail Sales or Services, General; Building Materials or Garden Equipment and Supplies Retailer	A	A	
Convenience Store	A		
Duplicating or Quick Printing Service; Private Postal or Mailing Service	A	A	
Pawn Shop			
Service Station, Gasoline	C		§5.4.6.R
Truck Stop			
Vehicle Sales	C	C	§5.4.6.M
Heavy Duty Truck or Commercial Vehicle Dealer; Manufactured Home Dealer	C	C	§5.4.6.M
Vehicle Parts, Accessories or Tire Store	A	A	

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
 Planning Commission Recommendation November 9, 2020

Table 5.4-1, Johns Island Maybank Highway Overlay Zoning District Use Table			
Uses	Mixed-Use (MU) District	Limited Commercial (LC) District	Conditions
RETAIL OR PERSONAL SERVICES			
Consumer Convenience Service	A	A	
Hair, Nail, or Skin Care Service	A	A	
Job Training or Placement Service	A	A	
Personal Improvement Service	A	A	
Physical Fitness or Health Club	A	A	
Tattoo Facility			
Services to Buildings or Dwellings	A	A	
Landscaping and Horticultural Service	A	A	
VEHICLE AND WATERCRAFT STORAGE			
Vehicle Storage	C	C	§5.4.6.P
Impound Yard			
Towing Facility			
Boat Ramp			
Community Dock			
Commercial Dock			
Marina			
WHOLESALE SALES			
Wholesale Sales	A		
Clay or Related Products, Construction Material Wholesaler	A		
Flower, Nursery Stock or Florists' Supplies Wholesaler	A		
Petroleum Wholesaler			
INDUSTRIAL			
INDUSTRIAL SERVICES			
Laundry, Dry Cleaning, or Carpet Cleaning Plant			
Photo Finishing Laboratory			
Research and Development Laboratory	A	A	
Scrap and Salvage Service			
MANUFACTURING AND PRODUCTION, GENERAL			
Artisan and Craftsman	A	A	

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
 Planning Commission Recommendation November 9, 2020

Table 5.4-1, Johns Island Maybank Highway Overlay Zoning District Use Table			
Uses	Mixed-Use (MU) District	Limited Commercial (LC) District	Conditions
Manufacturing and Production	A		
Aircraft Manufacturing and Production, including Related Parts			
Chemical Manufacturing and Production			
Clay or Related Products, Furniture, Cabinets or Related Products, Toy or Artwork, or Wood Products Manufacturing and Production	A	A	
Microbrewery	C	C	§5.4.6.Q
Pulp Mill or Paper Mill, Rendering Plant			
Slaughter House and Meat Packing			
Stone or Shell Products Manufacturing and Production			
WAREHOUSE AND FREIGHT MOVEMENT			
Warehouse and Distribution Facility			
Container Storage Facility			
Freight Forwarding Facility			
Fuel Storage Facility			
Grain Terminal and Elevator			
Stockpiling of Sand, Gravel, or other Aggregate Materials			
Storage or Manufacturing of Weapons or Ammunition			
OTHER USES			
RECYCLING SERVICES			
Recycling Center			
Recycling Collection Facility	A	A	
RESOURCE EXTRACTION/MINING			
Resource Extraction/Mining			
TRANSPORTATION			
Aviation			
Private Air Strip			
Railroad Facility			
Sightseeing Transportation, Land or Water	A		
Taxi or Limousine Service	A		
Urban Transit System	A	A	
Water Transportation			

§ 5.4.6 Use Conditions

A. Hemp Production and Processing

1. **Industrial Hemp License.** Evidence of the appropriate South Carolina Department of Agriculture Industrial Hemp License (Grower or Processor) shall be submitted with all Site Plan Review applications.
2. **Approval from South Carolina Department of Agriculture.** Evidence of the appropriate South Carolina Department of Agriculture Hemp Grower or Processor application approval shall be submitted with any Site Plan Review Application.

B. Agricultural Sales or Service, Retail Sales or Service, General, and Building Materials or Garden Equipment and Supplies Retailer

Tractor-trailer containers are prohibited in outside storage areas.

C. Single-Family Attached Dwellings

Single-Family Attached Dwellings shall be subject to the following standards:

1. **Number of Attached Units in a Single Structure.**
No single structure may contain more than eight Single-Family Attached Dwellings.
2. **Density/Intensity and Dimensional Standards.**
Single-Family Attached Dwellings shall comply with the applicable Density/Intensity and Dimensional Standards of this Article.
3. **Accessory Structures.**
All Accessory Structures shall be located on the same Lot as the associated principal Single-Family Attached Dwelling and shall be for the private use of the property occupant(s). A minimum interior setback of three feet is required between an Accessory Structure and the interior lot lines, provided that an Accessory Structure may be located on one of the zero lot lines when constructed of a material finish matching the Dwelling Unit exterior or when the Accessory Structure is the same height and materially a part of a fence or wall.
4. **Design Standards.**
 - a. *Front Façade.* The front façade of a Single-Family Attached Dwelling may not include more than 40 percent garage wall area.
 - b. *Roof.* The roof of each Single-Family Attached Dwelling must be distinct from the other through either separation of roof pitches or direction, or other variation in roof design.
 - c. *Windows.* At least 10 percent of the area of each façade that faces a street must be comprised of windows.

D. Short-Term Rentals, Commercial Guest House (CGH)

Short-Term Rentals, Commercial Guest House (CGH) use shall comply with the requirements of Article 6.8 of this Ordinance as regulated for the Community Commercial (CC) Zoning District.

E. Affordable Dwelling Units

1. Affordable Dwelling Units in the Mixed-Use (MU) District shall comply with the requirements of Article 6.4.19 of this Ordinance as regulated for the Community Commercial (CC) Zoning District.
2. Affordable Dwelling Units in the Limited Commercial (LC) District shall comply with the requirements of Article 6.4.19 of this Ordinance as regulated for the Neighborhood Commercial (NC) Zoning District.

F. Family Home

A Family Home, as defined in this Ordinance, does not require compliance with the Site Plan Review procedures contained within this Ordinance.

G. Community Residential Care Facility

A Community Residential Care Facility that provides care for nine or less persons shall be considered a Family, and is an allowed use in all Zoning Districts pursuant to the Fair Housing Act, Section 800 [42 U.S.C. 3601].

H. Outdoor Recreation and Entertainment

Any structure or activity use area established in connection with Outdoor Recreation and Entertainment uses shall have a vegetated land use buffer of not less than 50 feet from any property that contains a residential use or is located in an agricultural, residential or office Zoning District, except where such property line abuts a street, in which case the front setback established for the Zoning District shall apply.

I. Special Events

Special Events established as a principle use in the MU and LC Districts shall comply with the requirements of Article 6.7 of this Ordinance as regulated for the Community Commercial (CC) Zoning District.

J. Utility Service, Major

1. Sewage Disposal Facility, Water and Sewage Treatment Facility, Water Storage Tank, and Electric or Gas Power Generation Facility

Vegetated Buffers. Any structure established in connection with a Water Storage Tank, Water and Sewage Treatment Facility, Sewage Disposal Facility, or Electric or Gas Power Generation Facility shall have a vegetated buffer of not less than 50 feet from any property line, in compliance with the Chapter 9 buffer standards of this Ordinance.

2. Utility Substation, Electrical or Telephone Switching Facility, Sewage Collector or Trunk Line, Utility Pumping Station, and Water Main

- a. *Vegetated Buffers for Structures Less than 120 Square Feet.* Above ground structures that have a cumulative area of 120 square feet or less, associated with underground utilities such as meters, which are necessary for maintenance and monitoring, shall have a vegetated buffer of 10 feet from all property boundaries, in compliance with Chapter 9 buffer standards of this Ordinance;
- b. *Vegetated Buffers for Structures Greater than 120 Square Feet.* Above ground structures that have a cumulative area of greater than 120 square feet established in connection with a Utility Substation, Electrical or Telephone Switching Facility, Sewage Collector or Trunk Line, or Utility Pumping Station shall have a vegetated buffer of 25 feet from all property boundaries, or the minimum setback of the overlay district, whichever is greater; and
- c. *Storage of Vehicles and Equipment.* The accessory storage of vehicles and equipment on the premises shall be prohibited in the LC District.

K. Restaurants

All proposed Restaurants serving beer or alcoholic beverages located within 500 feet of the property line of a lot in a residential Zoning District or a lot containing a residential use shall require review and approval in accordance with the Special Exception procedures of this Ordinance. Distances shall be measured from the nearest property line of the subject parcel to the nearest property line of a lot containing a residential use or located in a residential Zoning District.

L. Heavy Construction Service or General Contractor, Special Trade Contractor, Consumer and Commercial Repair Service, and Vehicle Repair or Service

All materials and equipment associated with this use shall be entirely screened from view of adjacent properties, and public or private right-of-ways, by the use of solid fencing or appropriate landscaping.

M. Commercial or Industrial Machinery or Equipment, Construction Tools or Equipment, Heavy Duty Truck or Commercial Vehicle Rental or Leasing, Vehicle Rental or Leasing, and Vehicle Sales and Heavy Duty Truck or Commercial Vehicle Dealer; Manufactured Home Dealer

1. Vehicles must be located behind a commercial or office building as viewed from the facing street frontage.

2. In the LC District, Vehicle Sales, Dealers, Rental, and Leasing uses, including all associated vehicle storage areas, must not exceed one acre in size; otherwise, these uses are prohibited.

N. Self-Service Storage Facility

1. **Performance Standards**

a. **Street Frontages and Mixed-Use Development**

- i. In the MU District, a Self-Service Storage Facility shall be part of a mixed-use development where the ground floor use facing street frontages (entire street frontage) are commercial or office space independent of the Self-Service Storage Facility business.
- ii. In the LC District, a Self-Service Storage Facility shall be either:
 - a) Part of a mixed-use development where the ground floor use facing street frontages (entire street frontage) are commercial or office space independent of the Self-Service Storage Facility business, or
 - b) Part of a mixed-use development where the Self-Service Storage Facility business is contained within a separate building or buildings located behind a commercial or office space business as viewed from the facing street frontage.

b. **Front Setback**

All structures, including the accessory manager's office/residence must be set back a minimum of 25 feet from the right-of-way or the district minimum setback, whichever is greater.

c. **Side and Rear Buffers/Screening**

- i. *Office, Commercial or Industrial Zoned Lots.* Where projects abut lots zoned office, commercial, or industrial, no side and rear setbacks are required.
- ii. *Residential Zoned Lots.* Where sites abut residentially zoned properties, buildings adjacent to the perimeter must face inward with their doors away from such areas.

d. **Building Lengths and Access**

To ensure ease of access for emergency vehicles, no building shall exceed 300 feet in length. Spaces between ends of buildings shall be at least 30 feet.

e. **Accessory Office/Apartment**

One management office and/or accessory residence shall be permitted.

f. **Parking and Circulation**

- i. *Entrance.* Project entrances shall be 30 feet in width.
- ii. *Roadway Widths.* Roadway widths on interior drives shall be at least 24 feet in width where buildings face and open onto such drives on only one side. Where buildings face and open onto drives on both sides, widths of such drives shall be at least 34 feet.
- iii. *Turning Radii.* Turning radii, whether provided at the terminus of interior drives or at points between buildings, shall be at least 30 feet to provide for the maneuverability of emergency vehicles.

g. **Signs**

Signs shall comply with the requirements contained in this Article and Chapter 9 of this Ordinance.

2. **Operating Conditions**

a. **Commercial Activities**

The manufacture or sale of any commercial commodity or the provision of any service from the premises is prohibited.

b. **Commercial Repair Activities**

Commercial repairs of vehicles autos, boats, motors, furniture, or other items on the premises are prohibited.

c. **Storage of Flammable Substances**

Storage of flammable chemical substances within the complex is prohibited.

d. **Open Storage**

Open storage of automobiles vehicles and boats is permitted only where such areas are screened to comply with Landscaping, Screening and Buffer requirements contained in Chapter 9 of this Ordinance.

O. Liquefied Petroleum Gas Dealers

The amount of storage for Liquid Petroleum Gas Dealers shall be limited to 40,000 gallons per site.

P. Vehicle Storage

Open storage of vehicles and boats shall be entirely screened from view of adjacent properties, and public or private right-of-ways, by the use of solid fencing or appropriate landscaping.

Q. Microbrewery

1. *Barrel Capacity.* A Microbrewery shall have a maximum capacity of 5,000 barrels per year; otherwise, this use shall comply with the Special Exception procedures contained in this Ordinance.
2. *On-Site Consumption.* A Microbrewery shall require review and approval in accordance with the Special Exception procedures of this Ordinance if they allow on-site consumption of beer or alcoholic beverages in conjunction with the Microbrewery use or an Accessory Use.
3. *Proximity to Residential District or Use.* A Microbrewery shall require review and approval in accordance with the Special Exception procedures of this Ordinance if they are located within 500 feet of the property line of a lot in a residential Zoning District or a lot containing a residential use. Distances shall be measured from the nearest property line of the subject parcel to the nearest property line of a lot containing a residential use or located in a residential Zoning District.
4. *Accessory Uses and Structures.* All Accessory Uses and Structures shall comply with the requirements of Article 6.5 of this Ordinance.
5. *Special Events.* All Special Events uses shall comply with the requirements of Article 6.7 of this Ordinance.
6. *Third Party Vendors.* The Site Plan shall demonstrate that all required parking is maintained and remains unencumbered when third party vendors or structures are onsite.

R. Gasoline Service Stations

Gasoline Service Stations shall have a maximum of four fuel dispensing stations and a maximum of eight vehicle fueling positions (VFP). VFP's are the number of vehicles that can be fueled simultaneously at a gasoline service station.

§ 5.4.7 GENERAL DEVELOPMENT STANDARDS AND REQUIREMENTS (ALL DISTRICTS)

A. Residential Density

1. *Maximum Residential Density.* The Density/Intensity and Dimensional Standards listed in Table 5.4-3 of this Article shall apply to all properties in the MU District, and the Density/Intensity and Dimensional Standards listed in Table 5.4-5 of this Article shall apply to all properties in the LC District.
2. *Calculation of Residential Density.* Residential density shall be calculated by dividing the number of Lots/Dwelling Units on a site by the net area (in acres) of highland of the site on which the Lots/Dwelling Units are located. Net highland acres includes all acreage that is not below the Office of Coastal Resource Management Critical Line or identified as freshwater wetlands. Site Plan Review and Subdivision applications shall include all freshwater wetland metes and bounds, and total freshwater wetland acreage based on a United States Army Corps of Engineers (USACE) Approved Jurisdiction Determination (AJD). Accessory Dwelling Units (ADUs) are not included in the calculation of residential density.

B. Vehicle Access. Site Plan Review and Subdivision Plat applications shall include suitable access management plans demonstrating compliance with the requirements below:

1. *Driveway Separation.* For driveways directly accessing Maybank Highway, driveway separation shall be a minimum distance of 150 feet from the intersection of the right-of-way lines to the closest edge of driveways. Where the driveway separation is less than 150 feet the proposed driveway shall be designed as a right-in, right-out (RIRO) driveway and conform to the SCDOT standards for a Typical Right-in, Right-out Driveway Design.

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2. *Driveway Width.* Vehicular access from Maybank Highway and side (secondary) streets to properties shall be confined to access drives not exceeding 30 feet in width at the street line. Driveways which include a median strip to separate traffic flow in opposite directions shall not exceed 60 feet in width at the street line.
3. *Distance between Driveways.* The distance between the proposed driveway and an existing adjacent driveway shall be the greatest distance feasible.
4. *Limitation to Number of Driveways.* For parcels with a right-of-way frontage equal to or less than 150 feet in length, the development is limited to one driveway, unless single lane driveways are provided in accordance with § 5.4.7.B.5.; for parcels with a frontage greater than 150 feet and less than 300 feet in length, the development is limited to two driveways; for parcels with a frontage equal to or greater than 300 feet the development may have up to three driveways.
5. *Single Lane Driveways.* For parcels with a right-of-way frontage equal to or less than 150 feet in length, two single lane driveways may be allowed. Each single lane drive shall not exceed 15 feet in width at the street line, and the single lane driveway separation shall be a minimum distance of 100 feet from the intersection of the right-of-way lines to the closest edge of driveways.
6. *Corner Lots.* Notwithstanding the above stated requirements, access drives on corner lots shall be located only on the side (secondary) street and such driveways shall be a minimum distance of 50 feet from the street intersection as measured from the edge of the intersecting roadway to the beginning of the driveway radius. However, the Zoning and Planning Director may require the access to be located on the primary street to avoid undue interference with, or hazard to, traffic on the roadways.
7. *Shared Access Requirements.* Shared access is encouraged between adjoining parcels. Driveways for all uses except Single-Family Detached Dwellings should be located in a manner where they can be shared between adjacent parcels as described below:
 - a. The applicant must request a shared access with the adjacent property if the adjacent property does not contain a Single-Family Detached Dwelling.
 - b. Shared access should be located along a common property boundary, if feasible.
 - c. If the owner of the adjacent parcel does not agree to share access, the applicant shall provide one of the following to the Zoning and Planning Department:
 - i. A letter from the adjacent property owner denying access; or
 - ii. If the adjacent property owner refuses to provide a letter, an affidavit that documents attempts that the applicant made to request shared access and that the neighboring property owner refused to provide a letter. If this subsection applies, a new curb cut is permitted on the subject parcel only with a recorded agreement that the property owner will allow adjacent properties to share access when developed and/or redeveloped.
 - d. Shared access agreements shall be recorded with the Register of Deeds (ROD) Office.
 - e. When right-of-way is dedicated pursuant to the map titled "Johns Island Maybank Highway Corridor Overlay Zoning District Infrastructure and Connectivity" as determined by the Zoning and Planning Director and the parcel is involved in a shared access agreement, the maximum building cover shall be 60 percent. When right-of-way is not dedicated, but the parcel is involved in a shared access agreement, the maximum building cover shall be 40 percent.
8. *Transportation Coordination.* Prior to the issuance of a Building Permit, the applicant must show conformance with all requirements included in Letters of Coordination from the South Carolina Department of Transportation (SCDOT), Charleston County Public Works Department, and the Charleston Area Regional Transportation Authority (CARTA).
9. *Infrastructure Connectivity Dedication Incentives.*
The map titled "Johns Island Maybank Highway Corridor Overlay Zoning District Infrastructure and Connectivity" identifies the approximate location of a future connector road and drainage system

intended to support improved vehicular circulation and stormwater drainage within the JO-MHC-O District. Property owners may be eligible for certain incentives (as listed in Tables 5.4-3 and 5.4-5 below) in exchange for dedicating 50-foot rights-of-way for one or more segments of the planned connector road and drainage system as shown on the map titled "Johns Island Maybank Highway Corridor Overlay Zoning District Infrastructure and Connectivity" to the City of Charleston. These incentives shall only apply upon submittal of documentation that such rights-of-way have been dedicated to and accepted by the City of Charleston. If the 50-foot right-of-way is dedicated and accepted, it does not have to be constructed by the property owner unless it is used as access to the development.

C. Traffic Impact Analysis

All Site Plan Review applications for properties within the JO-MHC-O District shall comply with the requirements of Article 9.9, Traffic Impact Studies. Compliance with Art. 9.9, Traffic Impact Studies, is also required for Subdivision Plat applications that meet the thresholds contained in Art. 9.9.

D. Pedestrian Access and Sidewalks/Multi-Use Paths

1. *On-Site Pedestrian Access.* On-site pedestrian access shall comply with the requirements of this Ordinance and shall be included in site design illustrating access linkage to existing sidewalks, adjacent parcels, and within the development area. At-grade and grade-separated pedestrian walkways shall provide direct connections from the street to the main entrance and to abutting properties. Pedestrian walkways shall be designed and located in a manner that does not require pedestrians to walk through parking lots or within driveways.
2. *Maybank Highway Sidewalk and Multi-use Path Requirements.* A minimum 10-foot wide concrete sidewalk shall be required for properties in the Mixed Use (MU) District and a minimum 10-foot concrete multi-use path shall be required for properties in the Limited Commercial (LC) District. Sidewalks and multi-use paths must comply with the requirements listed below and the right-of-way buffer requirements contained in this Article. This Section applies when properties are developed or redeveloped in accordance with Sec. 3.7.1 of this Ordinance.
 - a. The minimum 10-foot wide concrete sidewalk or multi-use path, located as shown in Figures 1 and 2, as applicable, shall extend the length of the entire property frontage;
 - b. Where applicable as determined by the Zoning and Planning Director, asphalt surface material may be used in place of concrete to mitigate damage to existing trees that are to remain;
 - c. The property owner shall obtain and submit all required encroachment permits as part of the Site Plan Review application;
 - d. The property owner shall provide written documentation from the City of Charleston that the City will maintain the sidewalk or multi-use path upon approval of Certificates of Occupancy. This requirement is applicable regardless of whether the sidewalk or multi-use path is located in a right-of-way or within an easement. Such documentation shall be submitted as part of the Site Plan Review application;
 - e. If the sidewalk or multi-use path is located on private property, the property owner shall record an easement for the safe movement of pedestrians and the maintenance of the sidewalk; and
 - f. Sidewalks/multi-use path shall be installed and inspected prior to issuance of Certificate(s) of Occupancy.
3. *All Other Roads Sidewalk Requirements.* Sidewalk requirements for all roads other than Maybank Highway in the Overlay Zoning District shall comply with the sidewalk requirements of Chapter 9 of this Ordinance.

E. Signs

All signage must comply with the requirements of this Section in addition to the applicable requirements of Article 9.11, Signs, of this Ordinance.

1. Free-standing Signs

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- a. All new freestanding signs are to be designed as monument signs.
 - i. Signs shall have a maximum height of 10 feet and a maximum size of 40 square feet.
 - ii. Shared free standing signs shall be allowed with a maximum height of 12 feet and a maximum size of 60 square feet.
 - b. All sign illumination:
 - i. Illuminated signs located adjacent to any residential area shall be controlled so as not to create excessive glare to properties within adjacent residential areas.
 - ii. Electronic Copy Readerboard signs and Billboards are prohibited.
 - iii. No illumination that simulates traffic control devices or emergency vehicles shall be used.
 - iv. All illumination must be from a steady, stationary light source.
 - v. Internal Illumination:
 - a) Internally illuminated signs must be constructed of routed aluminum or similar opaque material so that only letters, numbers, and/or logos are illuminated.
 - b) Signs shall not have light reflecting backgrounds or letters.
 - c) All finishes shall be a matte finish.
 - vi. External Illumination:
 - a) Illumination shall be from a steady stationary light source, shielded and directed solely at the sign.
 - b) Light sources to illuminate signs shall be shielded as to not cause glare hazardous to pedestrians or vehicle drivers or so as to create a nuisance to adjacent properties.
 - c) The intensity of light shall not exceed twenty (20) footcandles at any point on the sign face.
 - d) The color of light sources to illuminate signs shall be white.
 - e) Signs shall not have light-reflecting backgrounds or letters.
 - c. Nonconforming Signs
 - i. Any Nonconforming sign located on property within this Overlay Zoning District that was legally permitted on or before [Date of Adoption of this Overlay Zoning District] that does not comply with the standards set forth in this Article must come into compliance with the requirements of this Article prior to the issuance of a Zoning Permit for: (1) a new business on the property; and/or (2) all changes other than re-facing and the required addition of pole covers as described in subsection ii below.
 - ii. A Nonconforming Sign may be re-faced without complying with the requirements of this Article provided a pole cover is added to the existing poles pursuant to the following requirements:
 - a) The pole cover shall be at least one-third the width of the sign cabinet; and
 - b) The pole cover shall be at least one-third the overall height of the pole sign, however the pole cover shall not exceed 8 feet in height.
2. Wall Signs
- a. The maximum size of a wall sign shall be in accordance with Table 9.11.4, Wall/Façade Signs, of this Ordinance;
 - b. In new multi-tenant developments, such as shopping centers and office parks, all tenant signs are to be similar in type, color, font size, font style, and method of illumination; and
 - c. New tenant signs in existing multi-tenant developments shall be consistent with the type and method of illumination of existing tenant signs.

F. Special Stormwater Requirements

All construction activities occurring on properties within the JO-MHC-O District may be subject to Special Protection Area stormwater design criteria as described in the most recent edition of the *Charleston County Stormwater Program Permitting Standards and Procedures Manual*.

§ 5.4.8 MIXED-USE (MU) DISTRICT

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The MU District includes JO-MHC-O Overlay Zoning District properties grouped in three noncontiguous mixed-use nodes, as shown on the map titled “Johns Island Maybank Highway Corridor Overlay Zoning District.” These mixed-use nodes currently consist of high and medium intensity commercial and residential uses such as shopping centers, restaurants, pharmacies, multi-family dwellings, and various small retail and professional offices, along with a mix of vacant and undeveloped parcels. Two of the three mixed-use nodes are located at major crossroads that currently serve as established and recognized Johns Island commercial centers.

The MU District is intended for higher intensity commercial development and higher density residential uses than those allowed in the LC District of this JO-MHC-O District. Future development in this district should offer a wide variety of comparison and specialty goods and services to a population greater than the immediate neighborhood, additionally providing convenience goods and services to the surrounding neighborhoods. The MU District accommodates different types of compatible land uses close together in appropriate locations to shorten trips and facilitate alternative modes of transportation such as walking, biking, and public transportation. Mixed-use buildings with retail, service and other uses on the ground floor and residential units above the nonresidential space should be encouraged. The following regulations apply in addition to the requirements of Sec. 5.4.7, General Development Standards and Requirements (All Districts), of this Article to unincorporated parcels within the MU District as indicated on the map titled “Johns Island Maybank Highway Corridor Overlay Zoning District”:

A. Buffers

1. *Land Use Buffer.* The land use buffer and landscape requirements of Chapter 9 shall apply.
2. MU District Right-of-Way Buffers
 - a. *Buffer Requirement.* For properties with frontage on Maybank Highway, the right-of-way buffer shown in Figure 1 shall be required when such properties are developed or redeveloped in accordance with Sec. 3.7.1 of this Ordinance:
 - b. *Buffer Description.* The Maybank Highway right-of-way buffer shall be a minimum of 15 feet in depth, and include the following:
 - i. *Planting Strip Requirements.* A five-foot planting strip including:
 - a. Street trees and additional required plantings meeting the requirements of Table 5.4-2. All required trees and plantings shall be planted prior to the issuance of Certificate(s) of Occupancy; and
 - b. Street lights subject to the following street light requirements:
 - 1) Street lights shall be provided as shown in Figure 1 at locations to be determined by the Zoning and Planning Director, provided that property owners shall only be responsible for the street lights located in front of their properties;
 - 2) All required encroachment permits from the SC Department of Transportation shall be included as part of the Site Plan Review application;
 - 3) Property owners/applicants shall provide documentation stating that they shall be responsible for bearing the costs of the conduit(s) for the required street light(s) and coordinating with Dominion Energy to erect the lights prior to the issuance of Certificate(s) of Occupancy;

- 4) Street lights shall have Octagonal heads as defined by Dominion Energy’s light palette and shall be placed on seventeen-foot (17’) tall Fluted poles. Should these designs/light types no longer be in existence at the time of land development application, the Zoning and Planning Director shall determine the appropriate pedestrian scale fixture to be used;
- 5) Street lights shall be installed prior to issuance of Certificate(s) of Occupancy; and
- 6) The property owner shall provide written documentation that the development/redevelopment will comply with the above stated requirements. Such documentation shall be submitted as part of the Site Plan Review application.

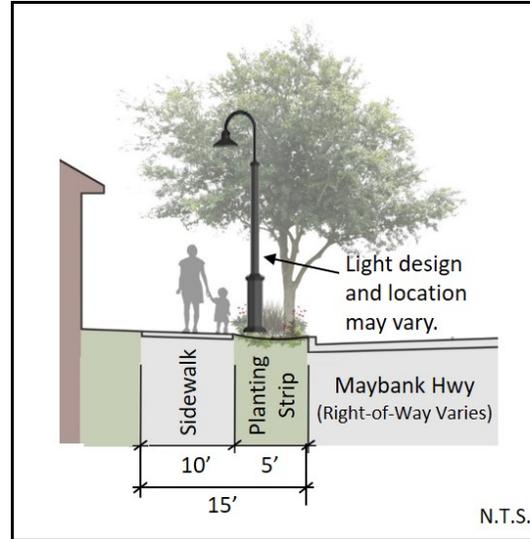


FIGURE 1
 RIGHT-OF-WAY BUFFER, MAYBANK HIGHWAY

- ii. *Sidewalk Requirements.* A 10-foot sidewalk shall be installed subject to the requirements of Sec. 5.4.7.D.2, Sidewalks/Multi-use Paths.

Table 5.4-2, Mixed-Use (MU) District Buffer Depth and Planting Schedule [1]	
STANDARD	MAYBANK HIGHWAY (JOHNS ISLAND)
MIN. BUFFER DEPTH [2] [3]	15 feet [4]
MINIMUM BUFFER LANDSCAPING (Plants per 100 linear feet)[5]	
Canopy Trees[6] [7]	3
Understory Trees (at least 50 percent evergreen)	3
Shrubs	25
Street Trees (may be counted toward canopy tree req.)[8]	2

All trees with a diameter breast height (DBH) of 6 inches or greater within buffers shall be preserved.

[1] All landscape and plant material shall comply with the minimum standards of Section 9.5.6 of this Ordinance.

[2] Buffers may be traversed by permitted driveways and pedestrian ways.

[3] As measured from existing back of curb, where applicable.

[4] Consisting of a five foot planting strip and ten foot sidewalk as illustrated in Figure 1.

[5] The Planning Director may waive/modify minimum buffer planting requirements when an undisturbed natural buffer exists that is the same depth and amount of plant material as that which is required.

[6] Bradford Pears cannot be used to fulfill any of the tree requirements of this Ordinance. Any exotic species which are proposed by the designer are subject to approval of the Planning Director.

[7] Only Live Oak trees can be used to fulfill canopy tree requirements.

[8] Street trees are trees planted in rights-of-way for the purpose of fulfilling these requirements. Any planting in rights-of-way must be approved by party(ies) authorized to grant encroachment.

3. If property is dedicated pursuant to Sec. 5.4.7.B.9, the connector road right-of-way buffer shall be eight feet.
4. Right-of-way buffer and sidewalk requirements for all other roads in the MU District shall comply Chapter 9 of this Ordinance.

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B. Density, Intensity, Dimensional, and Design Standards. The Density/Intensity and Dimensional Standards contained in Table 5.4-3 shall apply to properties in the MU District:

Table 5.4-3, Mixed-Use (MU) District Density/Intensity and Dimensional Standards		
STANDARD	WITHOUT 50' RIGHT-OF-WAY DEDICATION [6]	WITH 50' RIGHT-OF-WAY DEDICATION [7]
MAXIMUM RESIDENTIAL DENSITY [1]	8 dwelling units per acre	19 dwelling units per acre
MINIMUM LOT WIDTH	12 feet	12 feet
MINIMUM SETBACKS [2]	Equivalent to required buffers	Equivalent to required buffers
MAYBANK HIGHWAY RIGHT-OF-WAY BUFFER	15 feet	15 feet
OCRM CRITICAL LINE BUFFER	50 feet	50 feet
MAXIMUM BUILDING COVER [3]	30%	50%
MAXIMUM INDIVIDUAL BUILDING FOOTPRINT	15,000 square feet	20,000 square feet [4]
MAXIMUM BUILDING HEIGHT	35 feet/2½ stories	55 feet/5 stories [5]
<p>[1] To promote ownership or occupancy of affordable, quality housing, increased densities may be allowed pursuant to Sec. 5.4.6.E.</p> <p>[2] No building setback shall be less than eight feet.</p> <p>[3] When right-of-way is dedicated pursuant to the map titled "Johns Island Maybank Highway Corridor Overlay Zoning District Infrastructure and Connectivity" as determined by the Zoning and Planning Director and the parcel is involved in a shared access agreement pursuant to Sec. 5.4.7.B.7, the maximum building cover shall be 60 percent. When right-of-way is not dedicated, but the parcel is involved in a shared access agreement, the maximum building cover shall be 40 percent.</p> <p>[4] When right-of-way is dedicated pursuant to the map titled "Johns Island Maybank Highway Corridor Overlay Zoning District Infrastructure and Connectivity" as determined by the Zoning and Planning Director, building footprints of up to 50,000 square feet may be allowed if approved pursuant to the Special Exception procedures of this Ordinance.</p> <p>[5] When right-of-way is dedicated pursuant to the map titled "Johns Island Maybank Highway Corridor Overlay Zoning District Infrastructure and Connectivity" as determined by the Zoning and Planning Director, building heights of up to fifty five (55) feet and five (5) stories may be approved by the Zoning and Planning Director when the building meets all applicable setback and buffer requirements and the following conditions:</p> <ol style="list-style-type: none"> 1. The portion of the building exceeding thirty-five (35) feet in height is stepped back at least thirty (30) feet from the portion(s) of the building that meets the thirty-five (35) foot height requirement; and 2. The stepped portion is measured from the outside edge of the building that is parallel to Maybank Highway and, if applicable, from the outside edge of the building that is parallel to an adjacent parcel which contains or is zoned for single-family detached dwelling units. <p>[6] Vertical improvements shall not occur in the area identified for infrastructure connectivity as shown on the map titled "Johns Island Maybank Highway Corridor Overlay Zoning District Infrastructure and Connectivity" as determined by the Zoning and Planning Director.</p> <p>[7] These incentives shall only apply upon submittal of documentation that the 50-foot right-of-way has been dedicated to and accepted by the City of Charleston.</p>		

§ 5.4.9 LIMITED COMMERCIAL (LC) DISTRICT

The LC District includes JO-MHC-O Overlay Zoning District properties outside of the mixed-use nodes as shown on the map titled “Johns Island Maybank Highway Corridor Overlay Zoning District.” Existing uses in this district are primarily a mixture of light commercial, and vacant and undeveloped properties surrounded by established residential development.

The LC District is intended for lower intensity commercial development and lower density residential uses than those allowed in the MU District. This district should serve as a transition from the more intense mixed-use node and provide convenience goods and services to the surrounding neighborhoods as well as limited comparison shopping goods for a wider market. The range of comparison goods and services offered should be varied and include specialty retail stores, restaurants, and neighborhood-serving offices. Residential development most appropriate for this district includes small lot single-family dwellings and single-family attached dwellings, such as duplexes and townhouses, and triplexes and fourplexes/quadplexes. The following regulations apply in addition to the requirements of Sec. 5.4.7, General Development Standards and Requirements (All Areas), of this Article to unincorporated parcels within the LC District as indicated on the map titled “Johns Island Maybank Highway Overlay Zoning District”:

A. Buffers

1. *Land Use Buffer.* The land use buffer and landscape material requirements of Chapter 9 of this Ordinance shall apply.

2. LC District Right-of-Way Buffers.

a. Maybank Highway LC District Right-of-Way Buffer

i. *Buffer Requirement.* For properties with frontage on Maybank Highway, the right-of-way buffer shown in Figure 2 shall be required when such properties are developed or redeveloped in accordance with Sec. 3.7.1 of this Ordinance.

ii. *Buffer Description.* The Maybank Highway right-of-way buffer shall be 75 feet in depth and include the following:

- 0 – 25 feet from the property line at the right-of-way: a minimum 10-foot concrete multi-use path meeting the requirements of Sec. 5.4.7.D.2 and street lighting meeting the requirements of Sec. 5.4.9.A.3. This area is also reserved for future road widening.
- 25 – 50 feet from the property line at the right-of-way: Street trees and additional required plantings meeting the requirements of Table 5.4-4. All required trees and plantings shall be planted prior to the issuance of Certificate(s) of Occupancy.
- 50 - 75 feet from the property line at the right-of-way: This area is reserved for stormwater infrastructure; however, other temporary activities may occur in this area as approved by the Zoning and Planning Director. When stormwater detention ponds are located within the right-of-way buffer, they are only allowed in this portion of the buffer and they shall not occupy more than 25% of the entire right-of-way buffer area.

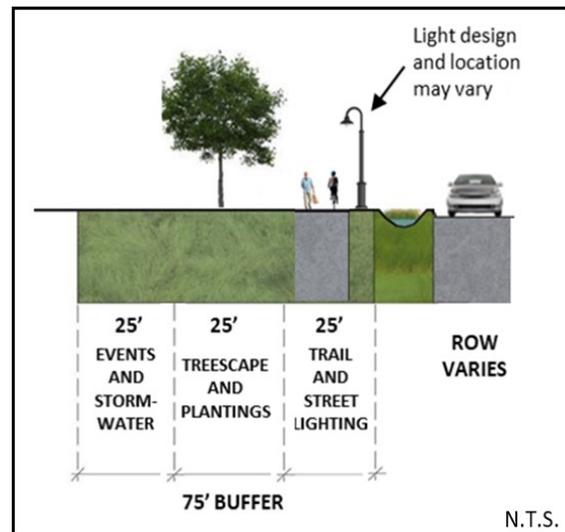


FIGURE 2
RIGHT-OF-WAY BUFFER, MAYBANK HIGHWAY

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Table 5.4-4, Limited Commercial (LC) District ROW Buffer Depth and Planting Schedule [1]	
STANDARD	MAYBANK HIGHWAY (JOHNS ISLAND)
MIN. BUFFER DEPTH [2] [3]	75 feet [4]
MINIMUM BUFFER LANDSCAPING (Plants per 100 linear feet)[5]	
Canopy Trees[6] [7]	3
Understory Trees (at least 50 percent evergreen)	4
Shrubs	30
Street Trees (may be counted toward canopy tree req.)[8]	2
All trees with a diameter breast height (DBH) of 6 inches or greater within buffers shall be preserved. [1] All landscape and plant material shall comply with the minimum standards of Sec. 9.5.6 of this Ordinance. [2] Buffers may be traversed by permitted driveways and pedestrian ways. [3] As measured from existing back of curb, where applicable. [4] Consisting of a minimum 10-foot multi-use path as illustrated in Figure 2. [5] The Zoning and Planning Director may waive/modify minimum buffer planting requirements when an undisturbed natural buffer exists that is the same depth and amount of plant material as that which is required. [6] Bradford Pears cannot be used to fulfill any of the tree requirements of this Ordinance. Any exotic species which are proposed by the designer are subject to approval of the Zoning and Planning Director. [7] Only Live Oak trees can be used to fulfill the canopy tree requirements. [8] Street trees are trees planted in rights-of-way for the purpose of fulfilling these requirements. Any planting in rights-of-way must be approved by party(ies) authorized to grant encroachment.	

- b. If property is dedicated pursuant to Sec. 5.4.7.B.9, the connector road right-of-way buffer shall be eight feet.
- c. Right-of-way buffer and sidewalk requirements for all other roads in the LC District shall comply with Chapter 9 of this Ordinance.

3. **Street Lights**

Street Light Requirements. Property owners shall install street lights, as shown in Figure 2, in accordance with the following standards:

- a. Street lights shall be placed in the first 25 feet of the right-of-way buffer at locations to be determined by the Zoning and Planning Director, provided that property owners shall only be responsible for the street lights located in front of their properties;
- b. All required encroachment permits from the SC Department of Transportation shall be included as part of the Site Plan Review application;
- c. Property owners/applicants shall provide documentation stating that they shall be responsible for bearing the costs of the conduit(s) for the required street light(s) and coordinating with electricity provider to erect the lights prior to the issuance of Certificate(s) of Occupancy;
- d. Street lights shall have Octagonal heads as defined by the electricity provider's light palette and shall be placed on 17-foot tall Fluted poles. Should these designs/light types no longer be in existence at the time of land development application, the Zoning and Planning Director shall determine the appropriate pedestrian scale fixture to be used;
- e. Street lights shall be installed prior to issuance of Certificate(s) of Occupancy; and
- f. The property owner shall provide written documentation that the development/redevelopment will comply with the above stated requirements. Such documentation shall be submitted as part of the Site Plan Review application.

B. Density, Intensity, Dimensional, and Design Standards. The Density/Intensity and Dimensional Standards contained in Table 5.4-5 shall apply to properties in the LC District:

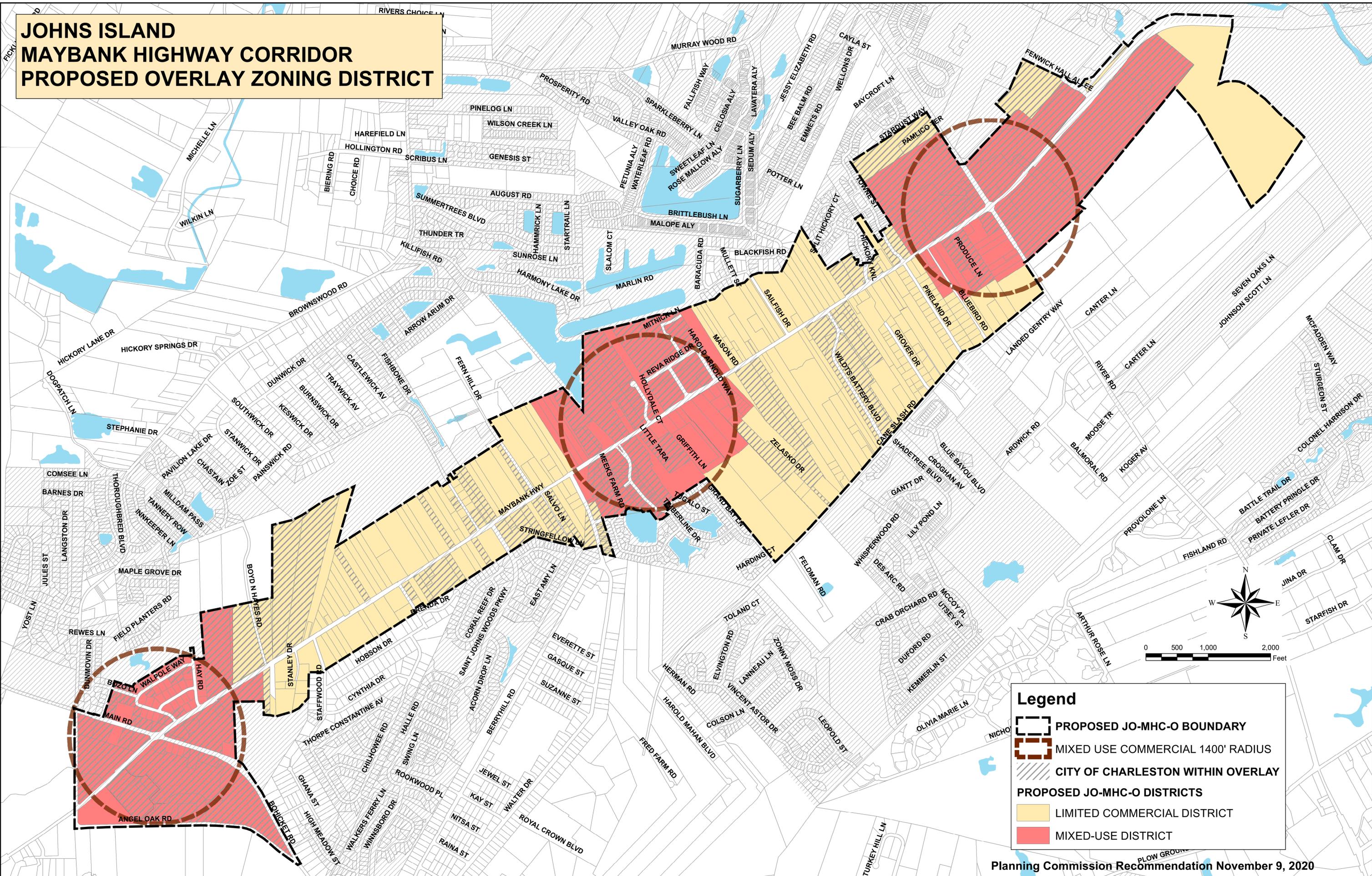
Draft Johns Island Maybank Highway Corridor Overlay Zoning District
 Planning Commission Recommendation November 9, 2020

Table 5.4-5, Limited Commercial (LC) District Density/Intensity and Dimensional Standards		
STANDARD	WITHOUT 50' RIGHT-OF-WAY DEDICATION [5]	WITH 50' RIGHT-OF-WAY DEDICATION [6]
MAXIMUM RESIDENTIAL DENSITY [1]	6 dwelling units per acre	12 dwelling units per acre
MINIMUM LOT WIDTH	12 feet	12 feet
MINIMUM SETBACKS [2]	Equivalent to required buffers	Equivalent to required buffers
MAYBANK HIGHWAY RIGHT-OF-WAY BUFFER	75 feet	75 feet
OCRM CRITICAL LINE BUFFER	50 feet	50 feet
MAXIMUM BUILDING COVER [3]	30%	50%
MAXIMUM INDIVIDUAL BUILDING FOOTPRINT	5,000 square feet	10,000 square feet [4]
MAXIMUM BUILDING HEIGHT	35 feet/2½ stories	45 feet/3½ stories
<p>[1] To promote ownership or occupancy of affordable, quality housing, increased densities may be allowed pursuant to Sec. 5.4.6.E.</p> <p>[2] No building setback shall be less than eight feet.</p> <p>[3] When right-of-way is dedicated pursuant to the map titled "Johns Island Maybank Highway Corridor Overlay Zoning District Infrastructure and Connectivity" as determined by the Zoning and Planning Director and the parcel is involved in a shared access agreement, the maximum building cover shall be 60 percent. When right-of-way is not dedicated, but the parcel is involved in a shared access agreement pursuant to Sec. 5.4.7.B.7, the maximum building cover shall be 40 percent.</p> <p>[4] When right-of-way is dedicated pursuant to the map titled "Johns Island Maybank Highway Corridor Overlay Zoning District Infrastructure and Connectivity" as determined by the Zoning and Planning Director, building footprints of up to 20,000 square feet may be allowed if approved pursuant to the Special Exception procedures of this Ordinance.</p> <p>[5] Vertical improvements shall not occur in the area identified for infrastructure connectivity as shown on the map titled "Johns Island Maybank Highway Corridor Overlay Zoning District Infrastructure and Connectivity" as determined by the Zoning and Planning Director.</p> <p>[6] These incentives shall only apply upon submittal of documentation that the 50-foot right-of-way has been dedicated to and accepted by the City of Charleston.</p>		

**Draft Johns Island
Maybank Highway
Corridor Overlay
Zoning District**

- Boundary Map**
- Infrastructure and
Connectivity Map**

JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR PROPOSED OVERLAY ZONING DISTRICT



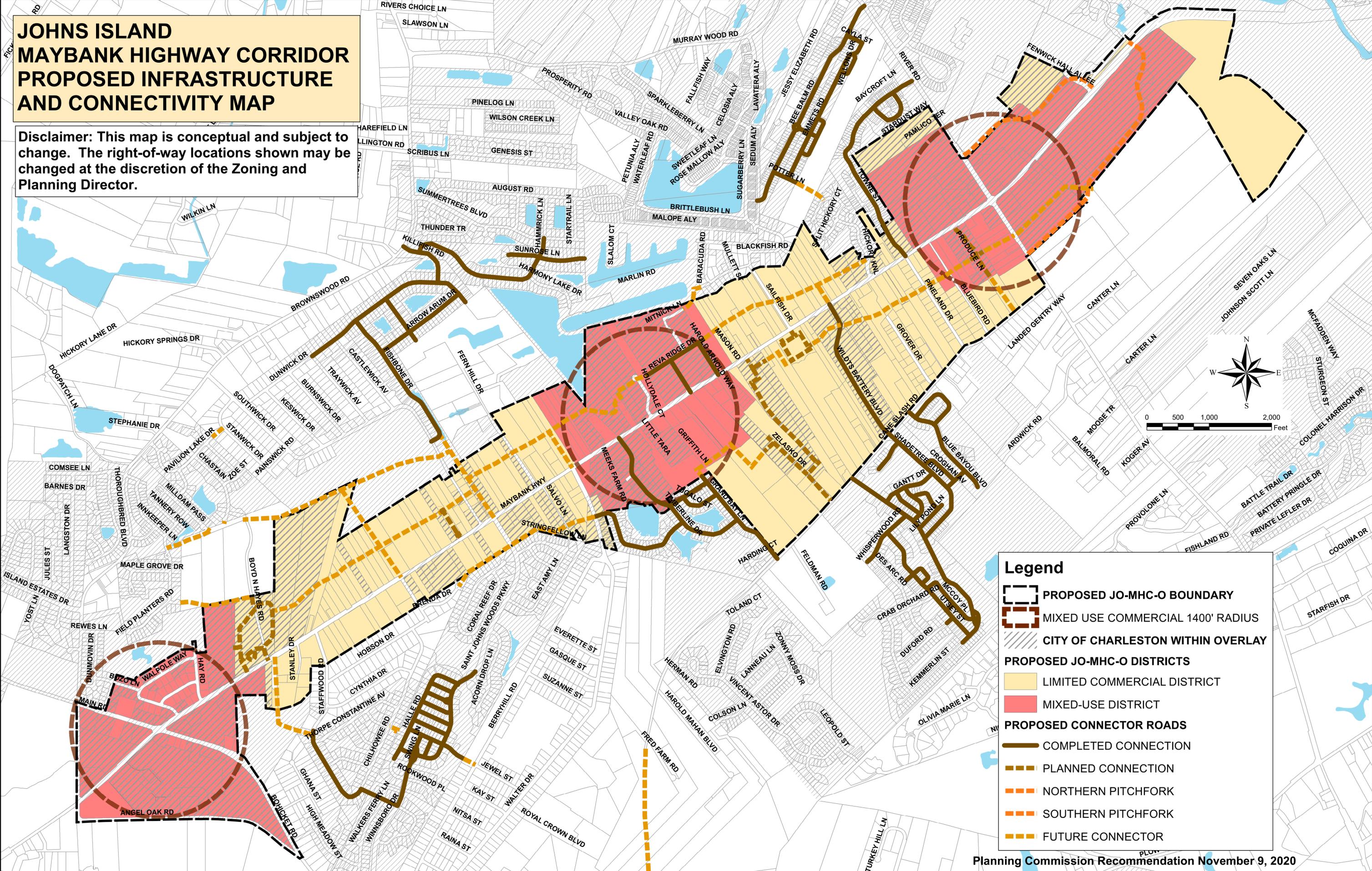
Legend

- PROPOSED JO-MHC-O BOUNDARY
- MIXED USE COMMERCIAL 1400' RADIUS
- CITY OF CHARLESTON WITHIN OVERLAY
- PROPOSED JO-MHC-O DISTRICTS**
- LIMITED COMMERCIAL DISTRICT
- MIXED-USE DISTRICT

Planning Commission Recommendation November 9, 2020

JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR PROPOSED INFRASTRUCTURE AND CONNECTIVITY MAP

Disclaimer: This map is conceptual and subject to change. The right-of-way locations shown may be changed at the discretion of the Zoning and Planning Director.



Legend

- PROPOSED JO-MHC-O BOUNDARY
- MIXED USE COMMERCIAL 1400' RADIUS
- CITY OF CHARLESTON WITHIN OVERLAY
- PROPOSED JO-MHC-O DISTRICTS**
 - LIMITED COMMERCIAL DISTRICT
 - MIXED-USE DISTRICT
- PROPOSED CONNECTOR ROADS**
 - COMPLETED CONNECTION
 - PLANNED CONNECTION
 - NORTHERN PITCHFORK
 - SOUTHERN PITCHFORK
 - FUTURE CONNECTOR

**Johns Island Maybank
Highway Corridor Overlay
Zoning District:**

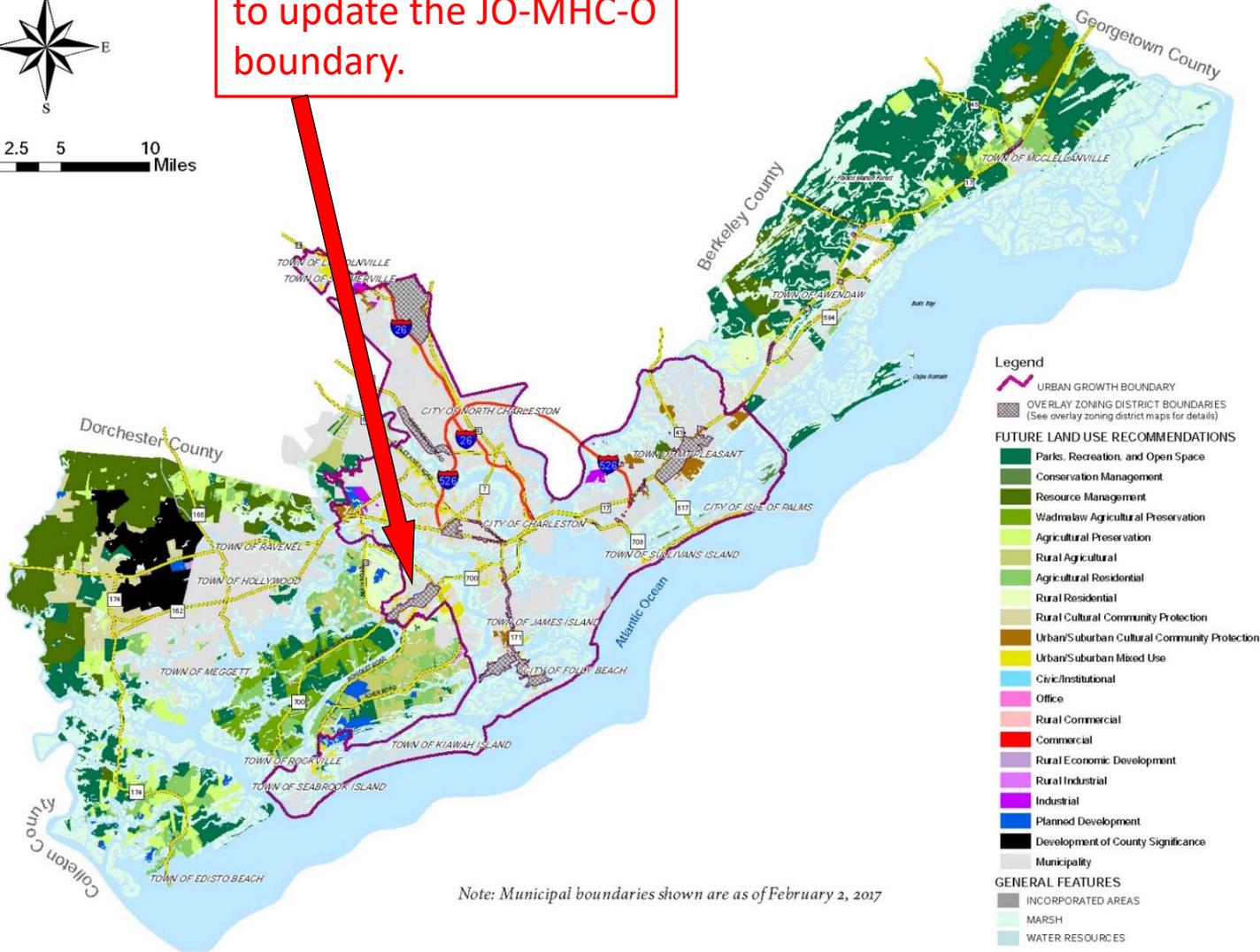
**Draft Comprehensive Plan
Amendments**

MAP 3.1.4: FUTURE LAND USE

Update Map 3.1.4:
Future Land Use
to update the JO-MHC-O
boundary.



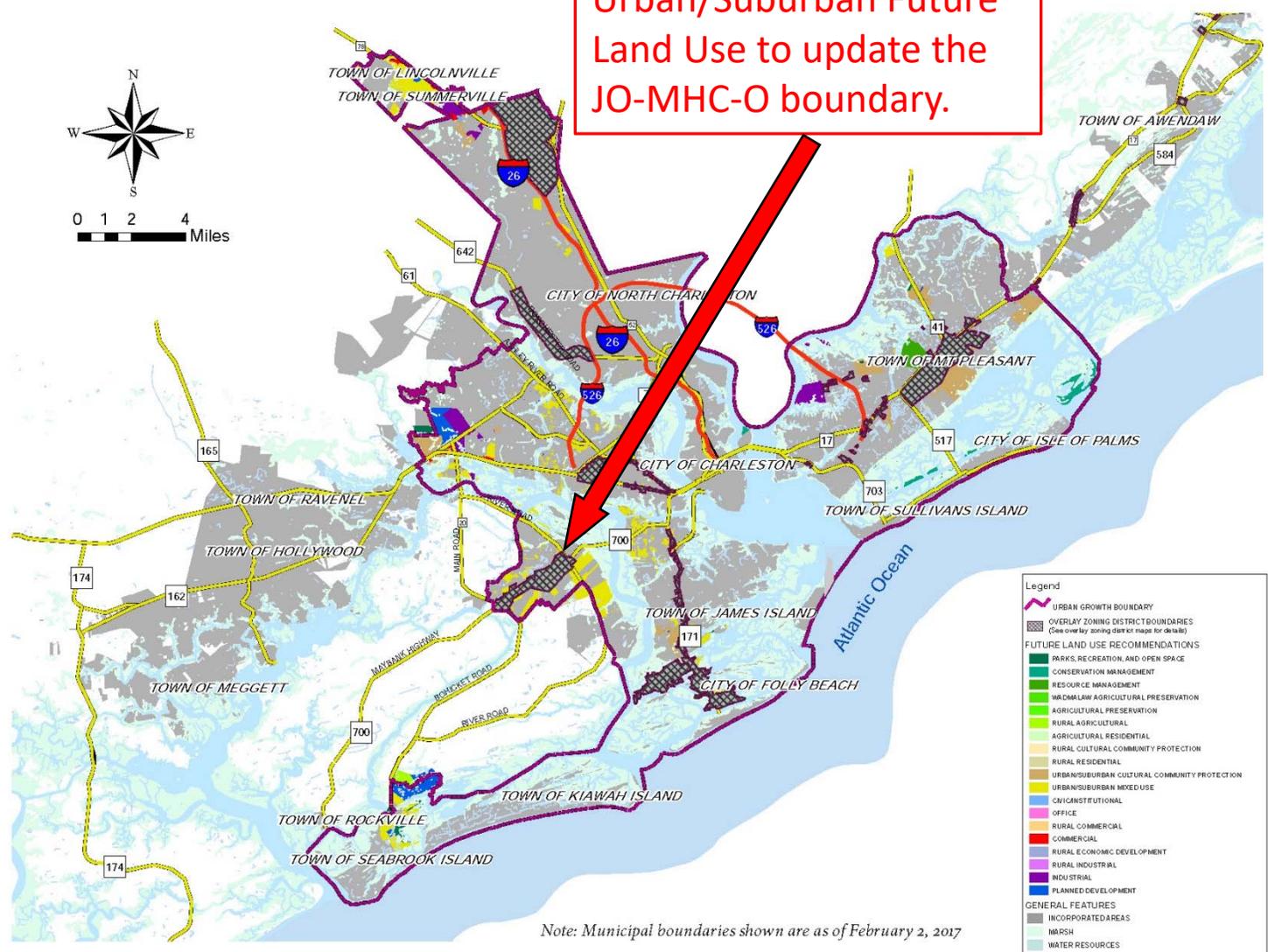
0 2.5 5 10
Miles



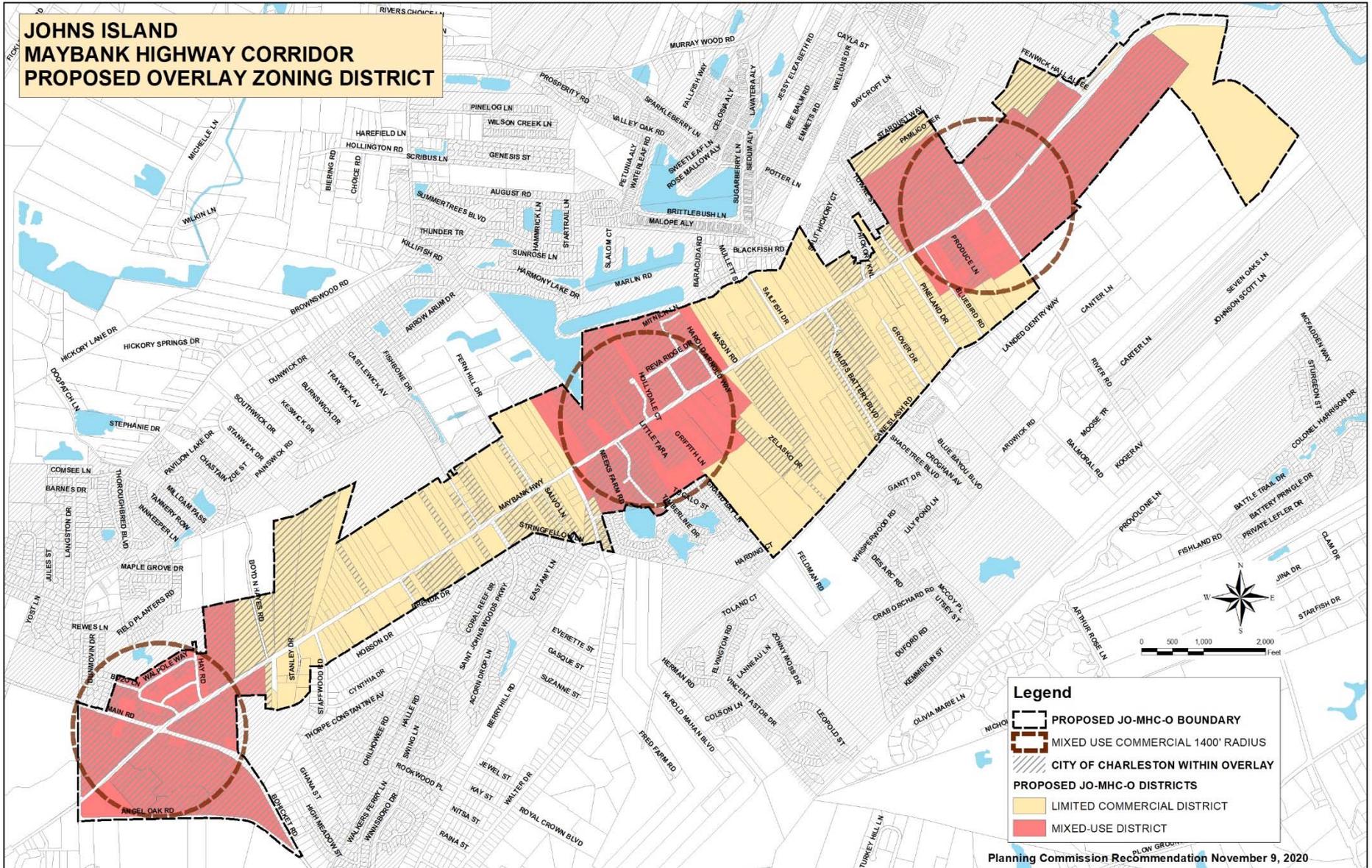
Note: Municipal boundaries shown are as of February 2, 2017

MAP 3.1.5: URBAN/SUBURBAN FUTURE LAND USE DETAIL

Update Map 3.1.5:
Urban/Suburban Future
Land Use to update the
JO-MHC-O boundary.



Update Map 3.1.8: Johns Island Maybank Highway Corridor Overlay Zoning District



Add Map 3.1.19: Johns Island Maybank Highway Corridor Overlay Zoning District Road Connectivity Map

JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR PROPOSED INFRASTRUCTURE AND CONNECTIVITY MAP

Disclaimer: This map is conceptual and subject to change. The right-of-way locations shown may be changed at the discretion of the Zoning and Planning Director.

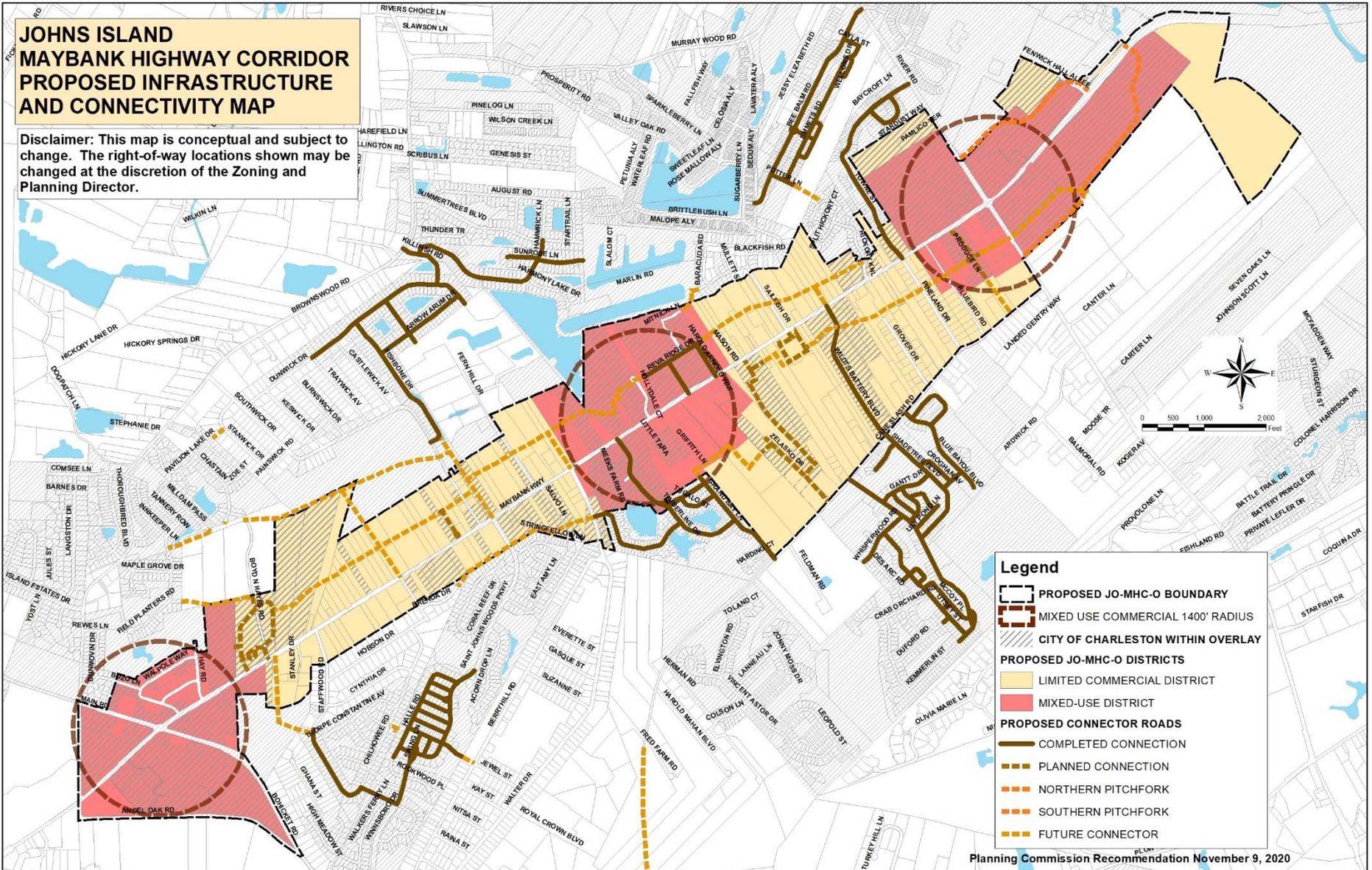
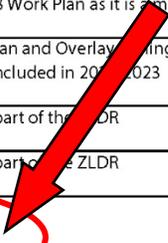


TABLE 3.9.1: PROJECT STATUSES FOR 2013-2014 FIVE-YEAR REVIEW PRIORITY RECOMMENDATIONS

Project	Intergovernmental Coordination	Project Status
Proposed Spring Grove Development (formerly East Edisto)	Towns of Hollywood, Meggett, and Ravenel	Development Agreement, Development of County Significance, and Form-Based Zoning District Adopted. Project development plan review will continue through 50-year life of the Development Agreement
Historic Resources Survey Update	State Historic Preservation Office	Completed in 2016
West Ashley Consistency Review	City of Charleston	Completed DuPont Wappoo Community Plan and Overlay Zoning District in 2016; participating in City of Charleston Ashley Master Plan project
Rural Area Cultural Community Protection Overlay Zoning District (Parkers Ferry Area)	Applicable jurisdictions and service providers	Completed in 2017
ReThink Folly Road Implementation	City of Charleston, Town of James Island, City of Folly Beach, and service providers	Plan adopted in 2016; implementation included in 2018-2023 Work Plan
ZLDR Comprehensive Review	All municipalities and service providers within Charleston County	Began in 2017; included in 2018-2023 Work Plan as it is a multi-year project
Urban/Suburban Area Cultural Community Protection Overlay Zoning District	Applicable jurisdictions and service providers	Completed Sol Legare Community Plan and Overlay Zoning District in 2018; other communities included in 2018-2023 Work Plan
Implementation of Urban/Suburban Future Land Use Designation	All municipalities within the Urban Growth Boundary	Included in 2018-2023 Work Plan as part of the ZLDR Comprehensive Review Project
Implementation of the Rural Residential Future Land Use Designation	City of Charleston and Town of Mount Pleasant	Included in 2018-2023 Work Plan as part of the ZLDR Comprehensive Review Project
Maybank Highway Corridor Overlay Zoning District (consistency review with the Johns Island Plan and extension onto James Island)	City of Charleston	Included in 2018-2023 Work Plan
Main Road (River Road to Maybank Highway, including Kitford Road Area)	City of Charleston	Included in 2018-2023 Work Plan
Mount Pleasant Overlay Zoning District - Sweetgrass Basket Stand Special Consideration Area	Town of Mount Pleasant	Included in 2018-2023 Work Plan
Reinforce the location of the Urban Growth Boundary	City of Charleston, City of North Charleston, Town of Mount Pleasant, and relevant service providers	Included in 2018-2023 Work Plan
Voluntary Agricultural and Forestal Areas Ordinance	Applicable jurisdictions	Included in 2018-2023 Work Plan

In Table 3.9.1, update the Project Status column to show "Completed in 2020"



Note: Projects listed above are in addition to staff's daily permitting and application review functions, such as zoning permit review and issuance; review of variance, special exception, appeals, rezoning, planned development, subdivision, and site plan review applications; preparation of staff reports for Board of Zoning Appeals, Planning Commission, and County Council; pre-application meetings with applicants and property owners; code enforcement; and addressing. The County Planning Department also provides similar services to the Towns of Kiawah Island, Meggett, Ravenel, and Rockville.

TABLE 3.9.2: 2018-2023 PRIORITY RECOMMENDATIONS

	Project	Recommended Time Frame*	Intergovernmental Coordination
1	ZLDR Comprehensive Review	2-3 Years	All municipalities and service providers within Charleston County
2	Maybank Highway Corridor Overlay Zoning District (consistency review with the Johns Island Plan and extension onto James Island) & Main Road (River Road to Maybank Highway, including Kitford Road Area)	1-2 Years	City of Charleston
3	Mount Pleasant Overlay Zoning District - Sweetgrass Basket Stand Special Consideration Area	1-2 Years	Town of Mount Pleasant
4	Urban/Suburban Area Cultural Community Protection Overlay Zoning Districts for Historic African-American Communities, as identified in the 2016 Charleston County Historic Resources Survey Update (e.g. Phillips, Snowden, Ten Mile, Grimball Farm, etc.)	2-3 Years	Applicable jurisdictions
5	Reinforce the location of the Urban Growth Boundary	1-2 Years	City of Charleston, City of Mount Pleasant, and Town of Mount Pleasant
6	Coordinate with Charleston County Economic Development Department to identify economic development opportunities in the Western area of Charleston County	1-2 Years	Towns of Hollywood and Mount Pleasant
6	Future Land Use Consistency Review in Highway 78 Area	2-3 Years	Town of Lincolnville, Berkeley County
7	Community Resiliency Element for Comprehensive Plan	2-3 Years	All municipalities and service providers within Charleston County
8	Voluntary Agricultural and Forestal Areas Ordinance	1-2 Years	Applicable jurisdictions
9	Wadmalaw Island Community Plan	2-3 Years	
10	Edisto Island ZLDR/Comprehensive Plan Amendments	2-3 Years	
11	Implementation of adopted Community Plans (e.g. DuPont Wappoo, Parkers Ferry, Sol Legare)	Ongoing	Applicable jurisdictions
12	ReThink Folly Road Implementation	Ongoing	City of Charleston, Town of James Island, City of Folly Beach, and service providers
13	2016 Charleston County Historic Resources Survey Update Implementation	Ongoing	Applicable jurisdictions and State Historic Preservation Office

Remove the Maybank Highway Corridor Overlay Zoning District project from Table 3.9.2.

*Time frame based on availability of adequate resources, to be directed and reviewed annually by County Council.

**Draft Johns Island
Maybank Highway Corridor
Overlay Zoning District:
ZLDR Related
Amendments**

Amend Sec. 8.4.2.A.4.j, as follows:

CHAPTER 8 | SUBDIVISION REGULATIONS

ARTICLE 8.4 PRELIMINARY PLAT

§8.4.2.A.4 Application; Requirements

A. Requirements

The following shall be submitted:

4. The following information shall be required on each plat:

- j. ***United States Army Corps of Engineers Jurisdictional Determination.*** A United States Army Corps of Engineers (USACE) jurisdictional determination is not required for approval of a subdivision plat application except: ***(1) as required for the Main Road Corridor Overlay Zoning District, James Island Maybank Highway Corridor Overlay Zoning District, and Johns Island Maybank Highway Overlay Zoning District; and (2)*** that an Accurate-Approved jurisdictional determination is required for areas located within proposed publicly dedicated rights-of-way and/or easements prior to Preliminary Plat approval. (...)

Amend Sec. 8.9.2.B.9, as follows:

CHAPTER 8 | SUBDIVISION REGULATIONS

ARTICLE 8.5 FINAL PLATS

§8.5.2.B Application;

B. The Final Plat Shall Show the Following:

9. ***United States Army Corps of Engineers Jurisdictional Determination.*** A United States Army Corps of Engineers (USACE) jurisdictional determination is not required for approval of a subdivision plat application except: ***(1) as required for the Main Road Corridor Overlay Zoning District, James Island Maybank Highway Corridor Overlay Zoning District, and Johns Island Maybank Highway Overlay Zoning District; and (2)*** that an Accurate-Approved jurisdictional determination is required for areas located within proposed publicly dedicated rights-of-way and/or easements prior to Final Plat approval. (...)

Amend Sec. 9.5.4, Table 9.5.4.A.3, Buffer Types by Roadway, as follows:

Table 9.5.4.A.3, Buffer Types by Roadway			
ROADWAY	BUFFER TYPE	ROADWAY	
Abbapoola Road	S4	Magwood Road	S3
Ashley Hall Road	S1	Main Road (Limehouse <u>Bridge</u> to Maybank Hwy.)	S5
Hwy. 61/Ashley River Road (Saint Andrews Boulevard to Sam Rittenberg Boulevard)	S1	Main Road (Bees Ferry Road to Limehouse <u>Bridge</u>)	S4
Hwy. 61/Ashley River Road (Sam Rittenberg Boulevard to Mark Clark Expressway)	S2	Manse Road	S4
Hwy. 61/Ashley River Road (Church Creek to Muirfield Parkway/MacLaura Hall Ave.) [1]	S5	Mary Ann Point Road	S3
Hwy. 61/Ashley River Road (Muirfield Parkway/ MacLaura Hall Avenue intersection to Charleston County Line)[1]	S6	Mathis Ferry Road [1]	S4
Bears Bluff Road	S5	Maybank Highway Corridor Overlay District [Johns Island]	[2]
Bees Ferry Road	S4	Maybank Highway Corridor Overlay Zoning District [James Island]	[4]
Belvedere Road	S4	Maybank Hwy (Main Road to Rockville)	S5
Betsy Kerrison Parkway [1]	S5	Meeting <u>Street</u>	S1
Bohicket Road [1]	S5	Murraywood Road	S4
Botany Bay Road	S4	Old Georgetown Road	S4

2. Buffer type as described in the Johns Island Maybank Highway Corridor Overlay Zoning District.

Public Input

From: [Joel Evans](#)
To: [Dan J. Frazier](#); [Andrea Pietras](#)
Subject: FW: Maybank Overlay district
Date: Tuesday, June 18, 2019 10:56:39 AM

From: A V Rawl [mailto:avrawl@aol.com]
Sent: Tuesday, June 18, 2019 10:18 AM
To: Joel Evans <JEvans@charlestoncounty.org>
Subject: Fwd: Maybank Overlay district

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For your info.
Sent from my iPhone

Begin forwarded message:

From: ggpine@aol.com
Date: June 18, 2019 at 9:01:52 AM EDT
To: avrawl@aol.com, ajohnson@charlestoncounty.org
Subject: Maybank Overlay district

I attended the meeting last night presented by the County and City. It looks like our governments are continuing down the path of making poor decisions. The decision to make Maybank 3 lanes form River rd to the bridge was a poor decision, now they are talking about a lane reversal scheme to fix the 3 lane problem they created....what a disaster. Now they are talking about right in and right out for new development on Maybank. Think about that, you will have to turn right out of an establishment and can not turn left into an establishment, so you have to find a place to turn around then drive back so you can turn right into it. Think how much this will add to the congestion on Maybank.

This city concept of three nodes, with the areas between them at a lower density "town and Country zoning", will not allow Johns Island to have all the much needed commercial services that we so desperately need. Why do we want to restrict commercial development on the only road that has all the infrastructure, Water, sewer, internet and gas, that commercial businesses need to operate.

Why cant we put a turn lane down the middle of Maybank? Why cant we have commercial development along all of Maybank. There is no Country on Maybank its our commercial business district!

It appears to many of us that this attempt to "fix things" will only make things worse, I hope it is not too late to change directions

Thank you for your service

George Gratzick MD

From: [Samuel Furr](#)
To: [Robin Lewis](#); [Holly, Aaron](#)
Cc: [Dan J. Frazier](#); [Anna B. Johnson](#); [Jenny C. Honeycutt](#); [Joel Evans](#); wagnerm@charleston-sc.gov; morganc@charleston-sc.gov; citizenservices@charleston-sc.gov; [Transportation Development](#)
Subject: Maybank Highway Overlay ordinance improvement and comments
Date: Wednesday, November 13, 2019 3:41:06 PM
Attachments: [JI diagram2.pdf](#)

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Dear Ms. Lewis and Mr. Holly,

I thank you and all the County and City staff who have been working hard and doing a good job on the Johns Island Maybank Highway Overlay District. As a owner of multiple properties and stake-holder on Maybank Highway, I have the following comments that I would like to see incorporated into the new Overlay ordinance.

1. Building heights in this urban LC district should allow for at least 3 1/2 stories (M zoning to be 5 1/2) in order to keep the footprints smaller and allow for more traditional, low-country higher ceilings. 35' is much too low for sloped roofs and this traditional language.
2. Do not require right in-and-out until there is a road to support u-turns. (A divided road with median of 40' min.)
3. More mixed use zoning throughout the urban corridor will allow market flexibility and can better support public transit and walkable neighborhoods. This is a proven and very successful urban use. Zone more mixed use areas at walkable intersections near neighborhoods. Perhaps "transit oriented development" every half mile. See attached diagram.
4. A 75' buffer is a suburban, car oriented buffer. A pedestrian friendly buffer should be much smaller to keep the scale of a walkable, family friendly neighborhood and not a super-highway. I do appreciate allowing swales and event areas but the landscape buffer should be incorporated within these and at least 25' eliminated.
5. Allow people to donate or plat right-of-ways without having to build roads and sidewalks to let non-developer and smaller property owners get the ball rolling on the new connector road system. Location of connector roads is good but I, and other smaller property owners, will not be able to pay to put in the road and drainage, however, the presence of these right-of-ways can facilitate this network happening sooner.
6. Regulating that the signs sit below 8' makes them anti-pedestrian friendly and blocks views to the landscaping, business establishments, as well as visibility to the road. They should be allowed to be put above the head-height and sight lines of pedestrians in a more traditional manner. Perhaps the signs could be smaller if elevated.

As a general comment, I believe that many people who have been interested in the Maybank Highway Overlay have been under the impression that it was about improving the public roadway. Many have expressed the idea that it should work for cars, bikes, pedestrians, and

transit as well as be a beautiful, verdant, connector of family neighborhoods. However, the focus has been primarily on zoning the private properties along a very needy highway. If there is any way to address this large discrepancy, it would be very helpful for all the stake-holders of the area.

Hopefully these items can be incorporated. I would be willing to come down and meet, explain, and help if necessary.

Sincerely,

Sam Furr

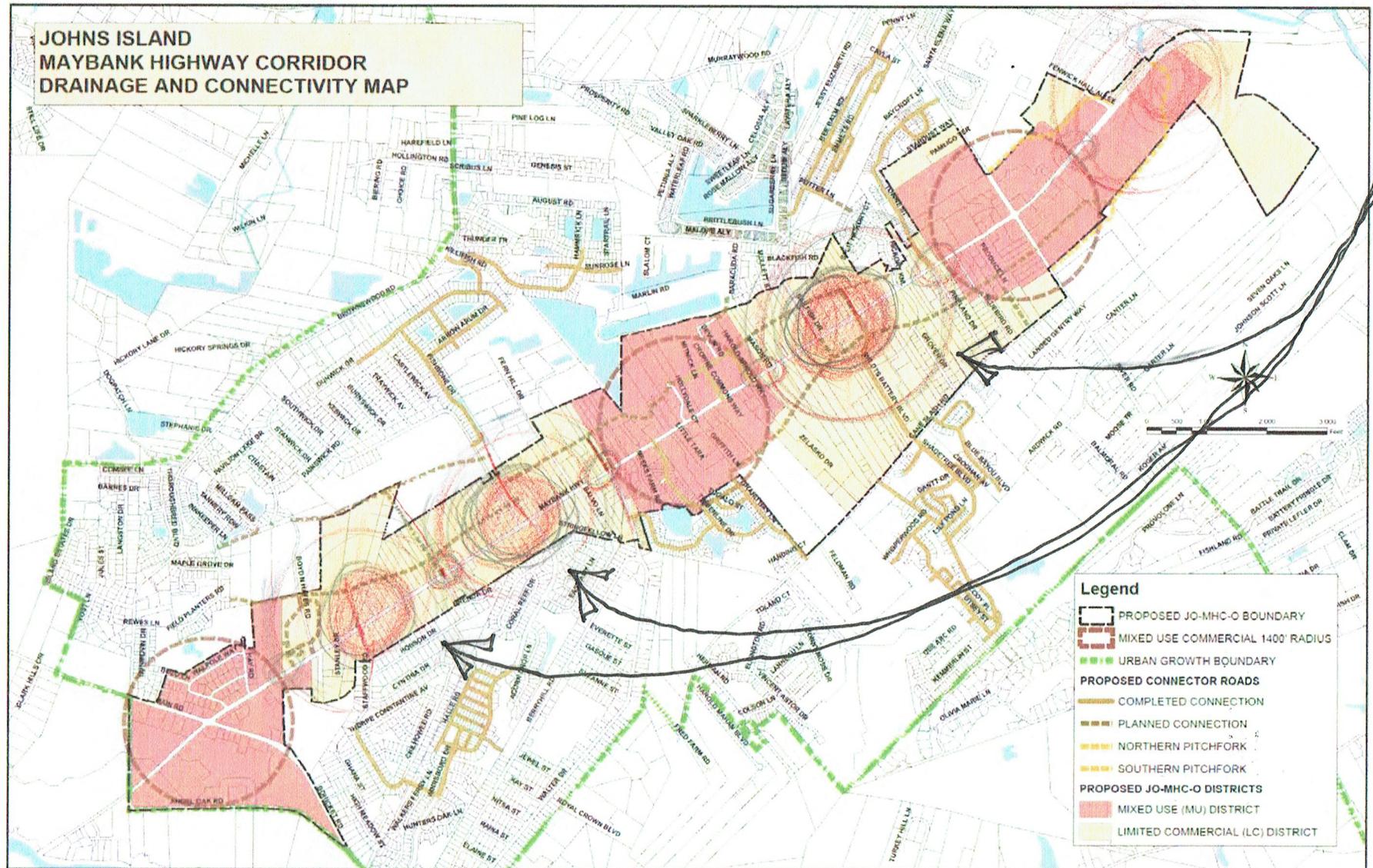
Samuel N. Furr, CNU, NCARB

Samuel Furr Architects
2968 Maybank Highway
Johns Island, SC 29455
843.559.5524

www.samfurrarchitects.com

JO-MHC-O DISTRICT PROPOSED CONNECTOR ROAD SYSTEM

MORE MIXED-USE (HIGHER DENSITY)
NODES AT CATCHMENT LOCATIONS
FOR TRANSIT AND WALKABLE-NEIGHBORHOODS



January 8, 2021

Mr. Joel Evans
Director, Zoning and Planning
Charleston County, SC

Dear Mr. Evans

We, the Board of Directors of the Villages of St Johns Woods Homeowners Association are writing to object to the recently adopted Johns Island Maybank Highway Corridor Proposed Infrastructure Connectivity Map showing a future connector road (gold dotted line) on Stringfellow Lane and adjacent to the back of several home lots on Coral Reef Drive in the Villages of St Johns Woods neighborhood. This would have a devastating effect on the value and marketability of 12 homes on Stringfellow Lane which is a one way, 12' wide alleyway serving as garage access for these homes. The Villages of St Johns Woods property boundary is directly adjacent to Stringfellow Lane. There is no room for a two lane connector road. If there was one, egress for these homeowners would be nearly impossible from their driveways and garages. The future connector road shown extends across the back of 12 Coral Reef Drive homes and would have similar negative impact on their home value and marketability. The connector road extends directly over a drainage pond now under construction across a commercial property being developed adjacent to St Johns Woods Parkway

The Board of Directors of the Villages of St Johns Wood Homeowners Association requests that the future connector road shown on Stringfellow Lane and across the back of Coral Reef Drive be removed from the Maybank Highway Corridor Proposed Infrastructure Connectivity Map for the reasons stated above. The Board also requests inclusion in any future planning zoning proposals affecting the Villages of St Johns Woods neighborhood.

Very Respectfully

The Villages of St Johns Woods HOA Board

Keith Gill, President contact jkeithgill@outlook.com *Keith Gill*

Beth Matenaer, Vice President *Beth Matenaer*

Warren Cavey, Treasurer *Warren Cavey*

Rebecca Stone-Danahy, Secretary *Rebecca Stone-Danahy*

Tim Hess, At Large Member *Tim Hess*







STRINGFELLOW

15



RESIDENTS ONLY
No Soliciting

DAVID GROCE



540.460.1861

January 14, 2021

Via Email: jevans@charlestoncounty.org

Joel H. Evans
Director, Zoning and Planning Department
Charleston County
4045 Bridge View Drive
North Charleston, South Carolina 29405

Re: Johns Island Maybank Highway Corridor Overlay Zoning District

Dear Mr. Evans:

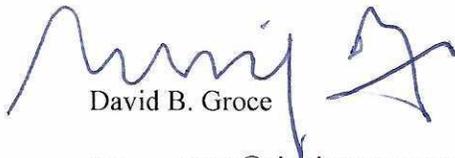
I am writing to express my strong opposition to the proposal before your department to re-zone, specifically, Stringfellow Lane, which runs through our subdivision, and more generally, other parcels in or adjacent to residential parcels proximate to Maybank Highway on Johns Island. The proposal, including the concept of so-called "connector roads" to alleviate the burden on Maybank Highway, is highly impractical, is unlikely to achieve your department's published goals for the District, and, if adopted, would cause unnecessary loss in value to hundreds of property owners.

Now, I recognize that Charleston County has had only a mere 19 years and 2 months to work on its plan for this District, but the plan that is to be considered at your meeting on January 19th, is absurdly flawed and should be abandoned. A new plan that provides for widening Maybank Highway to four lanes, plus turn lanes at relevant locations, plus sidewalks, plus measures to increase driver and pedestrian visibility and therefore safety, together with, for once, a commitment to enforce the county's sign ordinance, should be created, adopted, and implemented promptly (*i.e.*, less than 20 years).

Johns Island deserves better attention, planning, and resources.

Thank you.

Sincerely,



David B. Groce

cc: ccpc@charlestoncounty.org
Rick Jerue: jerue@charleston-sc.gov
Karl Brady: bradyk@charleston-sc.gov
Anna Johnson: ajohnson@charlestoncounty.org

DBG @ DAVIDBGRUCE.COM

3475 ACORN DROP LANE CHARLESTON, SC 29455

Niki R. Grimball

From: CCPC
Sent: Friday, January 15, 2021 11:09 AM
To: Niki R. Grimball
Subject: FW: Johns Island/Maybank Corridor Project

FYI!

Emily Pigott
Planner I
Charleston County Zoning & Planning
epigott@charlestoncounty.org
843-202-7225

> -----Original Message-----

> From: Diane <dianemorgan243@gmail.com>

> Sent: Thursday, January 14, 2021 7:04 PM

> To: CCPC <CCPC@charlestoncounty.org>

> Subject: Johns Island/Maybank Corridor Project

>

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> click links or open attachments from unknown senders or suspicious

> emails. If you are not sure, please contact IT helpdesk.

>

> Dear Members,

> I am writing to object to the portion of the proposal that would

> allow a "Connector Road" to be included on Stringfellow Lane in St Johns Woods .

> I am a homeowner who lives on that alley which is the only access

> to our backyards and garages.

> This would have a devastating effect on my property value ,

> quality of life and future. Being close to retirement and having

> worked very hard and currently working in the Community as an

> Emergency Room Nurse to have a home to spend my latter years in and

> have as an asset and possible use in the future or to pass on to my children this is devastating .

> How could you consider destroying a group of homeowners in this

> manner. I researched the pine filled property that backs up to

> Stringfellow when I purchased this property 3 years ago. I was aware

> it was privately owned but was zoned for possible specific things. It

> was not zoned for a noisy bar. Gas station etc that would have glaring

> lights and noise . It was not zoned that the quiet little alley up to our backyards and garages would be a
> CONNECTOR ROAD ! It should not be .

> Please be cognizant of the horrible financial repercussions to a group

> of homeowners if you pass this zoning .

> I can not sleep at night with this weighing on me. This is unnecessary

> and my neighbors feel the same but most are unaware .

> Thank you

> Diane C. Morgan

> 4105 East Amy Lane

> Johns Island .SC 29455

>

>

Niki R. Grimball

From: CCPC
Sent: Friday, January 15, 2021 11:10 AM
To: Niki R. Grimball
Subject: FW: Proposed zoning change for Stringfellow Lane in the Villages of St Johns Woods

Emily Pigott
Planner I
Charleston County Zoning & Planning
epigott@charlestoncounty.org
843-202-7225

> -----Original Message-----

> From: Linda Wood <linjdwood@gmail.com>
> Sent: Thursday, January 14, 2021 7:55 PM
> To: CCPC <CCPC@charlestoncounty.org>
> Subject: Proposed zoning change for Stringfellow Lane in the Villages
> of St Johns Woods

>

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> click links or open attachments from unknown senders or suspicious
> emails. If you are not sure, please contact IT helpdesk.

>

> I am a homeowner in the above named community with access to the back
> of my home through the alley called Stringfellow Lane. I would like to
> speak at the meeting on January 19 in opposition to this zoning change
> and the designation of Stringfellow Rd as a connector.

>

> Stringfellow Rd is a very narrow one-way alley used for two purposes:
> access to the garages, parking pads, and back doors for the houses
> along that alley and for trash and recycling pickup. It gets very
> little traffic. The homes that back up on Stringfellow are designed on small lots with very narrow front yards.

All access and

> functional "yard" space is in the back. Any change in this passageway would generate
> a danger to entry and exit from these homes in addition to excessive traffic and noise.
> Changing this tiny alley to a corridor would affect the quality of my
> life as my back yard is where I "live".

>

> I have lived along this alley for 15 years. In recent years the tree
> filled area across the alley and over the fence that delineates our
> neighborhood has been cleared and a Go Store It facility was built
> right behind my home. Although it is unsightly, it is at least a low
> use facility and fairly quiet. I am grateful that it is not a busy gas
> station or a Chick-fil-A! I oppose to any zoning change that would allow such a change behind these homes.

>

> In addition to the above reasons, I oppose this zoning change because
> of the affect it would have on the value of my property. I am 75
> years old. I made an investment in this property in a highly sought
> neighborhood. The affects of the proposed zoning changes with the

> Stringfellow corridor would cause my home to lose the specific things that make it so attractive, that is a quiet garden-like back yard.

>

> Please consider my comments in your decision.

>

> Thank you for your attention,

> Linda D Wood

> 4111 East Amy Lane

> Johns Island, 29455

> 843-860-5813

>

> Sent from my iPad

Niki R. Grimball

From: CCPC
Sent: Friday, January 15, 2021 4:01 PM
To: Niki R. Grimball
Subject: FW: May bank Highway Corridor overlay Zoning District

Emily Pigott
Planner I
Charleston County Zoning & Planning
epigott@charlestoncounty.org
843-202-7225

> -----Original Message-----

> From: Pauline Colbert <pauline_colbert@hotmail.com>
> Sent: Friday, January 15, 2021 1:00 PM
> To: CCPC <CCPC@charlestoncounty.org>
> Subject: May bank Highway Corridor overlay Zoning District

>

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> click links or open attachments from unknown senders or suspicious
> emails. If you are not sure, please contact IT helpdesk.

>

> I'm writing to you as a homeowner whose backyard is on Stringfellow
> Lane I vehemently oppose the proposed zoning plan for the obvious
> reasons of how it will affect the value of my property and quality of
> life. I've been a resident for 16 years, bought my lot when there
> were just lots of pine trees. I've worked hard all my life to afford this house and planned on a peaceful
retirement.

> I respectfully ask the council to take into consideration how these
> plans will affect so many Johns Island residents.

> Pauline Colbert
> 4095 E. Amy Lane
> Johns Island

>

> Sent from my iPad

Niki R. Grimball

From: CCPC
Sent: Tuesday, January 19, 2021 8:16 AM
To: Niki R. Grimball
Subject: FW: Maybank Highway

Emily Pigott
Planner I
Charleston County Zoning & Planning
epigott@charlestoncounty.org
843-202-7225

From: Allen Knapp <acknapp2012@gmail.com>
Sent: Saturday, January 16, 2021 6:09 PM
To: CCPC <CCPC@charlestoncounty.org>
Subject: Maybank Highway

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I am writing to you to ask you to act against approval of the Johns Island Maybank Highway Corridor Overlay Zoning District. This is on the Council agenda for a public hearing on Tuesday, January 19th.

I recognize that this proposal has benefits. The development standards seem to be beneficial at first glance and seem like they might actually improve the standard of living here on the Island.

However, there are two things that concern me:

First, I am looking at the current zoning versus the proposed zoning of some of the properties that are adjacent to the Villages at Saint Johns Woods. As I read it, the proposed zoning (Limited Commercial) will be more lenient than the current zoning (Commercial Transitional). Under the proposed zoning of these properties, facilities such as “retail sales, limited

manufacturing and production” would be permitted with a size of up to 20,000 square feet.

Secondly, I am concerned about a proposed connector road that could incorporate an existing one-way alley (Stringfellow Lane) and would be adjacent to about 22 properties in the Villages. For one thing, there may not be enough room for a two-way road without seriously affecting driver egress for many properties along Stringfellow. The plan also shows the proposed future connector road going right through a retention pond on a property that is currently under construction. It seems to me that very little planning was done with regard to this connector road.

Either of these two situations would have a devastating effect on the value and marketability of up to 22 homes in The Villages, and a corresponding negative effect on the tax base for the County and City of Charleston.

I spoke in opposition to this Zoning District Proposal at the November 9th Planning Commission meeting but it was nevertheless approved unanimously by the Planning Commission.

I respectfully urge you to take whatever steps you can now to prevent this from being passed by the County Council.

Sincerely,
Allen C. Knapp
4081 E Amy Lane
Johns Island, SC 29455

Niki R. Grimball

From: CCPC
Sent: Tuesday, January 19, 2021 8:16 AM
To: Niki R. Grimball
Subject: FW: Johns Island Maybank Highway Corridor Overlay Zoning District.

Emily Pigott
Planner I
Charleston County Zoning & Planning
epigott@charlestoncounty.org
843-202-7225

From: fern suzann <fern.suzann@gmail.com>
Sent: Saturday, January 16, 2021 6:37 PM
To: Anna B. Johnson <AJohnson@charlestoncounty.org>; CCPC <CCPC@charlestoncounty.org>
Subject: Johns Island Maybank Highway Corridor Overlay Zoning District.

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Sirs and Madam,

I am writing to you to ask you to act against approval of the Johns Island Maybank Highway Corridor Overlay Zoning District. This is on the Council agenda for a public hearing on Tuesday, January 19th.

I recognize that this proposal has benefits. The development standards seem to be beneficial at first glance and seem like they might actually improve the standard of living here on the Island.

However, there are two things that concern me:

First, I am looking at the current zoning versus the proposed zoning of some of the properties that are adjacent to the Villages at Saint Johns Woods. As I read it, the proposed zoning (Limited Commercial) will be more lenient than the current zoning (Commercial Transitional). Under the proposed zoning of these properties, facilities such as "retail sales, limited manufacturing and production" would be permitted with a size of up to 20,000 square feet.

Secondly, I am concerned about a proposed connector road that could incorporate an existing one-way alley (Stringfellow Lane) and would be adjacent to about 22 properties in the Villages. For one thing, there may not be enough room for a two-way road without seriously affecting driver egress for many properties along Stringfellow. The plan also shows the proposed future connector road going right through a retention pond on a property that is currently under construction. It seems to me that very little planning was done with regard to this connector road.

Either of these two situations would have a devastating effect on the value and marketability of up to 22 homes in The Villages, and a corresponding negative effect on the tax base for the County and City of Charleston. Johns Island has already been negatively impacted by prior zoning and building regulations - you cannot in good conscience approve this preventable huge debilitating impact to the residents of Johns Island.

A resident of Johns Island spoke in opposition to this Zoning District Proposal at the November 9th Planning Commission meeting but it was nevertheless approved unanimously by the Planning Commission.

I respectfully urge you to take whatever steps you can now to prevent this from being passed by the County Council.



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Niki R. Grimball

From: CCPC
Sent: Tuesday, January 19, 2021 8:17 AM
To: Niki R. Grimball
Subject: FW: Maybank Highway Corridor Overlay District project boundary

Emily Pigott
Planner I
Charleston County Zoning & Planning
epigott@charlestoncounty.org
843-202-7225

From: Jeanne williams <jeannewilliams@comcast.net>
Sent: Monday, January 18, 2021 9:24 PM
To: CCPC <CCPC@charlestoncounty.org>
Subject: Maybank Highway Corridor Overlay District project boundary

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To whom it may concern:

The overlay project is already causing severe distress to the adjacent property owners of which I am included. The City of Charleston has already allowed a developer to raise the elevation of his property on Produce lane to be elevated with hard pan fill dirt so that it is now 3-4 feet above my property and definitely impacts Maybank Highway as well since we are in extremely close proximity.. I have pictures and video to support this claim but the city of Charleston and Chris Morgan have ignored our pleas to stop the hundreds of loads from being dumped on this property and now we have experienced severe flooding as a result and their pond for overflow is within feet of Maybank Highway. After being sued for millions of dollars by the developer for not giving up our easement I countersued and won only to be informed by Judge Scarborough that unless I had millions to sustain their appeals the developers have endless money thus they always win. I am appealing to the county to please represent the people who voted you into office and help us to rectify this situation or at the very least consider this in the overlay project. This is a severe threat to Maybank Highway. Please help us.

Sincerely,
Jeanne Williams

Niki R. Grimball

From: CCPC
Sent: Tuesday, January 19, 2021 10:23 AM
To: Niki R. Grimball
Subject: FW: Johns Island Maybank Hwy Corridor Project

Emily Pigott
Planner I
Charleston County Zoning & Planning
epigott@charlestoncounty.org
843-202-7225

From: JACOB PALMER <jacob_palmer@comcast.net>
Sent: Tuesday, January 19, 2021 9:13 AM
To: CCPC <CCPC@charlestoncounty.org>
Subject: Johns Island Maybank Hwy Corridor Project

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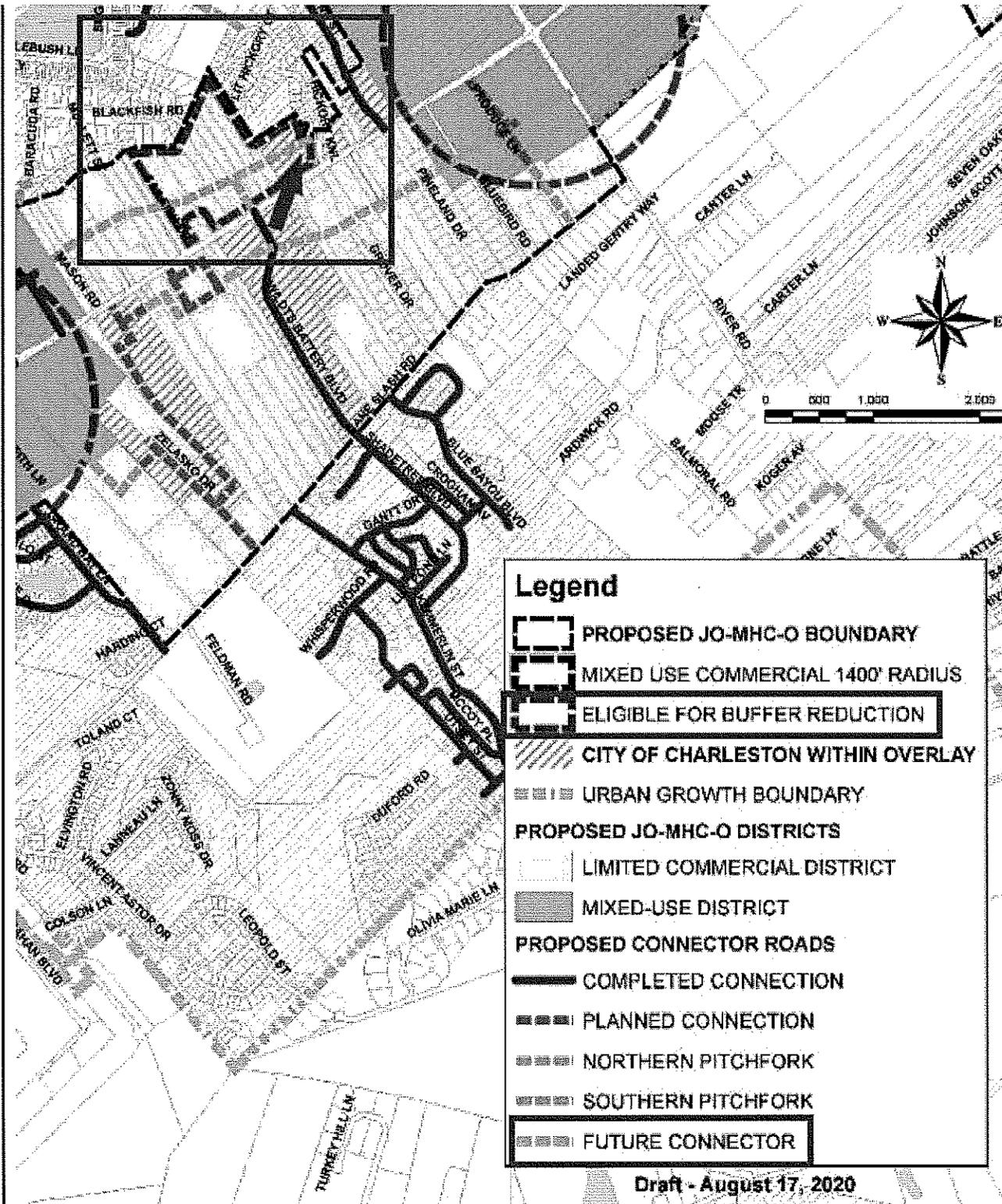
Hello,

I live in Barberry Woods which is just off of Maybank Hwy. I am located adjacent to where the map shows a "Future Connector". You can see where in the screenshot attached.

This is extremely concerning to me because this would put a through street up against my property which would cause concern for safety, noise issues, and a myriad of other problems. I understand development is necessary to reduce traffic and work with the increase of residents on the island, but I feel like this is not the best option.

I would like to know:

1. What is meant by "Future Connector"
2. How likely this specific part of the project is to be carried out
3. What the timeline is for this to be carried out



Niki R. Grimball

From: CCPC
Sent: Tuesday, January 19, 2021 12:38 PM
To: Niki R. Grimball
Subject: FW: Johns Island Maybank Highway Corridor Overlay Zoning District

Emily Pigott
Planner I
Charleston County Zoning & Planning
epigott@charlestoncounty.org
843-202-7225

From: John Hughes <hughesjp57@gmail.com>
Sent: Tuesday, January 19, 2021 11:55 AM
To: CCPC <CCPC@charlestoncounty.org>
Subject: Johns Island Maybank Highway Corridor Overlay Zoning District

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I have the following input regarding the Johns Island Maybank Highway Corridor Overlay Zoning District presentation:

Regarding the Johns Island Maybank Highway Corridor Current Zoning Map it is suggested that Key be made for the Legend to indicate what the County and City Zoning abbreviations stand for. For example, for Charleston City SR-1 means Single family (detached dwellings only) residential district.

Regarding the Johns Island Maybank Highway Corridor Proposed Infrastructure and Connectivity Map it is suggested (less expensive) pedestrian connectivity be considered too.

For example:

- A pedestrian bridge from Kay Street to the Pulte development (Arbors in St Johns Woods) walking trail.
- A pedestrian bridge from the Staffordshire neighborhood (Parcel ID 2790000558) to Haut Gap Middle School and the Johns Island County Library would improve access to those two county facilities.
- Add a sidewalk on Bohicket Road from Winnsboro Drive to Berryhill Road.

Regards, John Hughes
Coral Reef Drive
Johns Island, SC 29455